

Appendix 1

Public Consultation Summary Report

Urmston to Sale Water Park Cycle Improvements

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Executive Summary

Following a successful bid to Highways England, One Trafford are undertaking the design and construction of the Urmston to Sale Water Park Cycle Improvement Scheme. The aim of the improvements are to provide more attractive, safe, accessible and integrated cycling and pedestrian facilities, which will encourage higher levels of cycling/walking participation. To assist in the design process a consultation has been carried out on the draft proposals, which include upgrading the following routes:

- Dane Road Access
- River Mersey – Sale Water Park
- River Mersey – Bridgewater Canal
- River Mersey – near Carrington Spur
- Urmston (Stretford Rd) – NCN 62 bridge over the M60
- Sale to Chorlton

Another aim of the consultation was to determine the public opinion of a new bridge design at Jackson's Boat. The existing bridge at Jackson's Boat was built circa 1881 and carries a 1.6m wide pedestrian and cycle path over the River Mersey, which forms the boundary between Sale (Trafford) to the west and Chorlton (Manchester City Council) to the east. It should be noted that as of 25th January 2019 the existing bridge located at Jackson's Boat was added to the List of Buildings of Special Architectural or Historic Interest by the Secretary of State for Digital, Culture, Media and Sport. The bridge is listed at Grade II level.

The proposed bridge will form part of the improved cycle network. The bridge will tie in with new access arrangements at the west end of the bridge.

Three options are explored in this report:

1. Level of public support for bridge design proposal;
2. Preferred bridge design proposal; and
3. Proposed location of proposed bridge design.

Recommendations

Public Consultations were undertaken to give members of public an opportunity to share their opinions on the proposed scheme. Following analysis of the consultation results, the following is recommended:

- Provision of unbound surface treatment for the proposed routes to cater for all users including, equestrians, cyclists and pedestrians. With the exception of routes alongside River Mersey Sale Water Park and Carrington Spur. This is since these proposed routes are located along the top section of the embankments where there is varying available width and equestrians can utilise the wider paths alongside the river bed. It is therefore proposed to provide bound shared paths along the top of the embankments for the provision of cyclists and pedestrians only.
- Additional cycle route to be considered for design through Turn Moss playing fields.
- Existing bridge located at Jackson's Boat to remain. Proposed warren truss bridge or butterfly arch bridge to be constructed in location subject to agreement with Trafford Council, Manchester City Council and the Local Planning Authority. Proposed bridge design required to cater for equestrians as well as cyclists and pedestrians.

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1. Introduction

1.1. Background to the scheme

Situated in the south-western region of Greater Manchester, the Mersey Valley has a number of existing routes often shared by cyclists, walkers and equestrians for both recreation and commuting. Within the scheme extents there are numerous barriers to cycling in the form of security barriers, bollards and swing gates which impede accessibility into the area. In addition to this the M60 motorway severs the Mersey Valley dissecting and interrupting the routes at several locations.

1.2. Design Aspirations

The proposals for the overarching scheme can be divided into two categories. Category 1 concerns the improvements to eight existing routes between Urmston and Sale Water Park. These routes are as follows:

- 01 – Dane Road Access
- 02 – North Mersey: Sale Water Park
- 03 – South Mersey: Sale Water Park
- 04 – Mersey: Bridgewater Canal
- 05 – Bridgewater Canal Connection
- 06 – Mersey: Carrington Spur
- 07 – Urmston Link
- 08 – South East Access

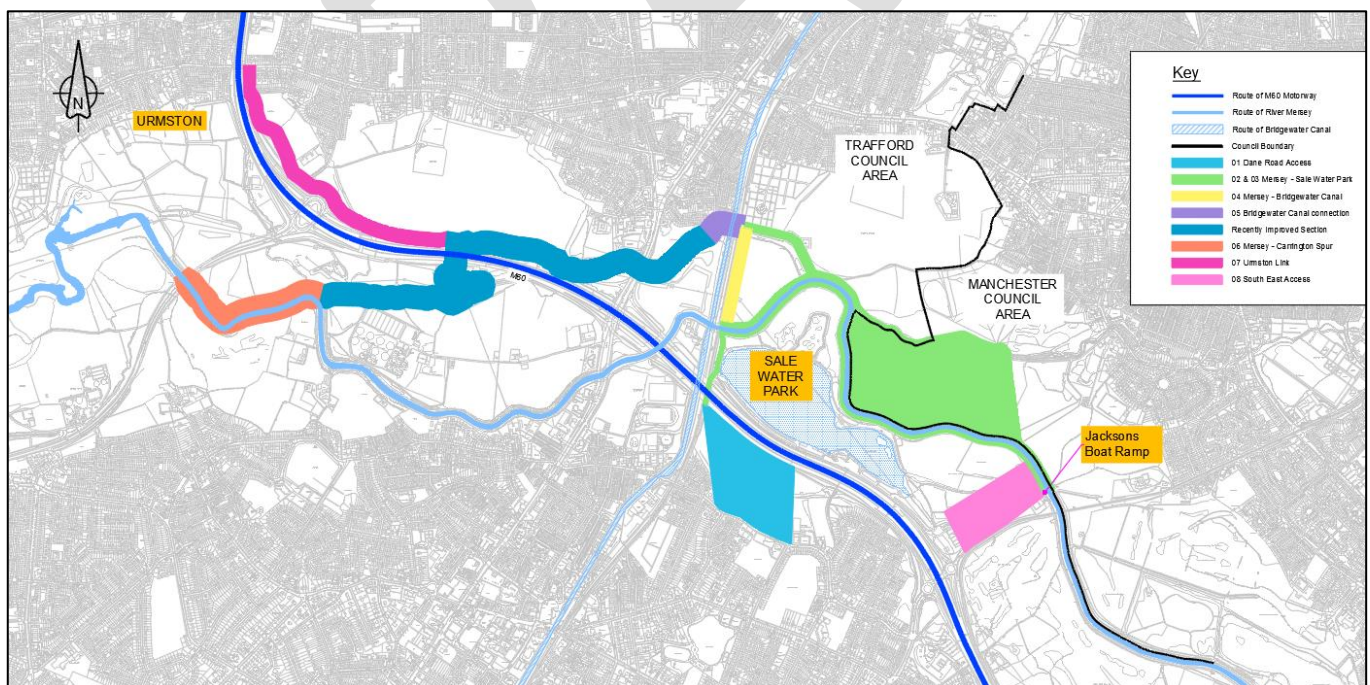
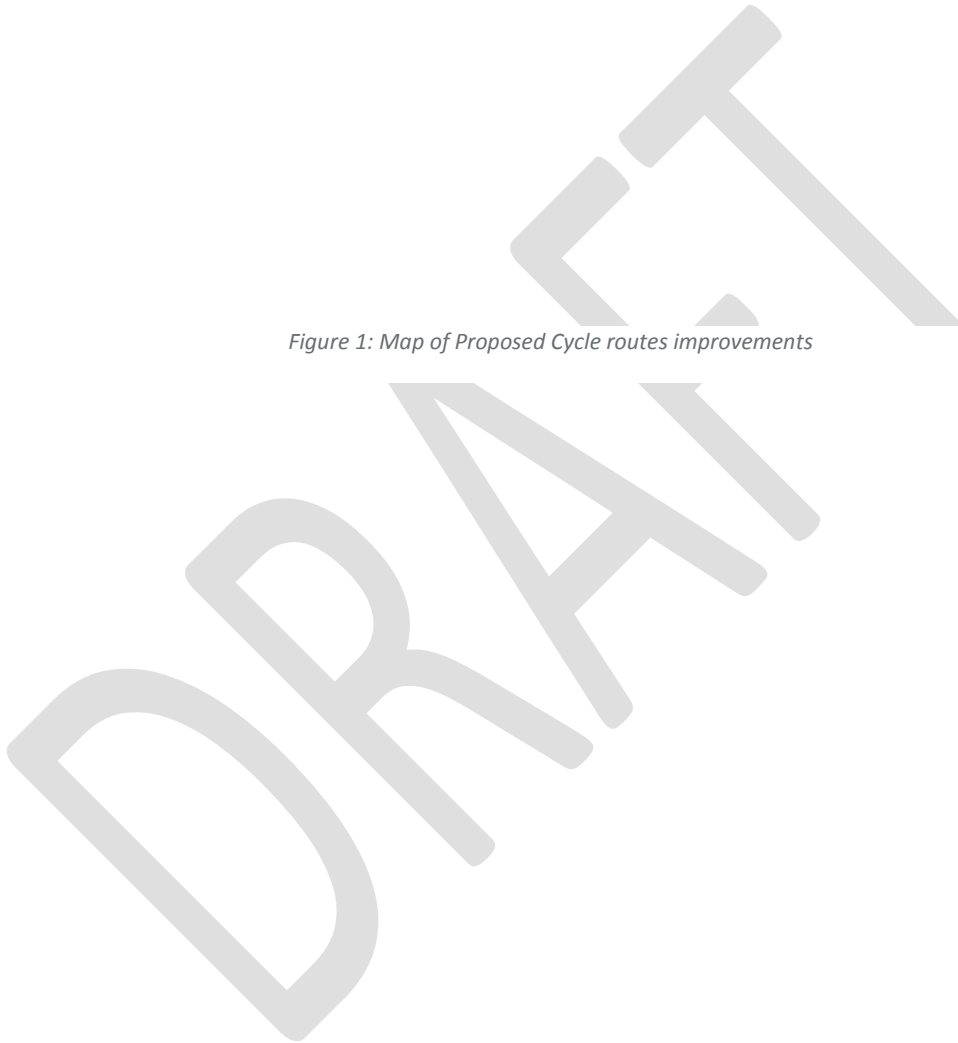


Figure 1: Map of Proposed Cycle routes improvements



Category 2 concerns the proposed accessibility improvements to the bridge at Jackson's Boat.

Jackson's Boat Bridge is a single span, steel truss girder bridge with arched overhead steel bracing and a timber deck. The structure comprises three spans. The length of the centre span is 30.6m, the northern approach span is 2.3m long and the southern approach span is 2.8m long.

The previous bridge assessment from December 2000 found that the structure was inadequate for full pedestrian loading (5kN/m^2 – rated as 3.8kN/m^2) in accordance with BD 21/01.

The current bridge is too narrow to meet current design guidance in accordance with 'Interim Advice Note 195/16 – Cycle Traffic and Strategic Road Network' from Standards for Highways. The document states in section 2.5.4 that deck width should allow for effect of parapets with minimum width of 3.5m.

The current width between parapets is 1.6m. This presents a safety risk to both pedestrians and cyclists. The cobbled surfaced ramp at the west end of the bridge is very steep and leads cyclists at speed into the road and car parking area for Jacksons Boat Pub, presenting an obvious safety risk to cyclists and those using the car park. The proposed bridge will significantly improve accessibility from Rifle Road for disabled and other mobility-impaired people.

It should be noted that subsequent to the consultation (25th January 2019) the existing bridge located at Jackson's Boat was added to the List of Buildings of Special Architectural or Historic Interest by the Secretary of State for Digital, Culture, Media and Sport. The bridge is listed at Grade II.

1.3. Aim of Public Consultation

The aim of the public consultation and online questionnaire was to establish the views of the local stakeholders on the proposed improvements to the cycle network between Urmston and Sale Water Park to assist in the final designs of the scheme. The consultation questionnaire can be seen in appendix A and the comments received can be seen in appendix B.

1.4. What the public consultation process entailed

Consultations and discussions were undertaken throughout the months of October and November 2018 to give representatives of various stakeholder groups the opportunity to share their opinions on the proposed scheme. These stakeholder groups are as follows:

- Trans Pennine Trail – National Trans Pennine Trail Office
- Police Community Officers
- Jackson's Boat Pub
- Impacted Land Owners
- Sustrans
- Local Ramblers associations
- Environmental Agency
- Trafford Cycle Forum
- Trafford Council
- Manchester City Council

A brief overview of these consultations is outlined below:

Public Consultation Event (Mersey Valley Visitor Centre) – 18th October 2018 (11:30 -19:00) & 19th October 2018 (10:30 – 14:30)

All comments received at this event have been included in the online consultation comments within Appendix A

Online Consultation (12th October 2018 – 9th November 2018)

During this period 232 responses were received. Of these, 220 were received via the online consultation and 12 were received via the post.

E-mail Responses

In addition to the formal consultation, a number of submissions were made via email, a summary of which is outlined below in Appendix C.

A detailed analysis of all responses is included within Chapter 2.

It should be noted that all public consultation was undertaken prior to 25th January 2019 when the existing bridge located at Jackson's Boat became a grade II listed structure.

2. Consultation Results Analysis

2.1. Introduction

The online Survey was open for participation for 28 days between 12th October 2018 and 9th November 2018, during which it received 232 responses to the multiple-choice questions. It is worth noting that the comments received during the public consultation event held on the 18th and 19th October 2018 were inputted onto the online survey. The full list of comments are within Appendix B.

Standard industry practice suggests that a mixed method analysis should be adopted to interpret the results from the online survey, due to the amount of multiple choice responses as well as additional comments.

The qualitative analysis allows the public to freely express their opinions on the proposed cycle scheme improvements while highlighting the crucial factors for further design. The quantitative analysis element of the research tests the reliability, relevance and precision of these factors.

2.2. Quantitative Analysis

The main body of the survey consisted of seven multiple choice questions:

Question 1 - Do you support the proposed cycle route improvements?

The responses to question 1 can be seen in Figure 2. 81% of the respondents answered 'yes' to supporting the proposed cycle route improvements. Conversely, 19% of respondents disagreed with the proposed scheme as they answered 'no' to this question.

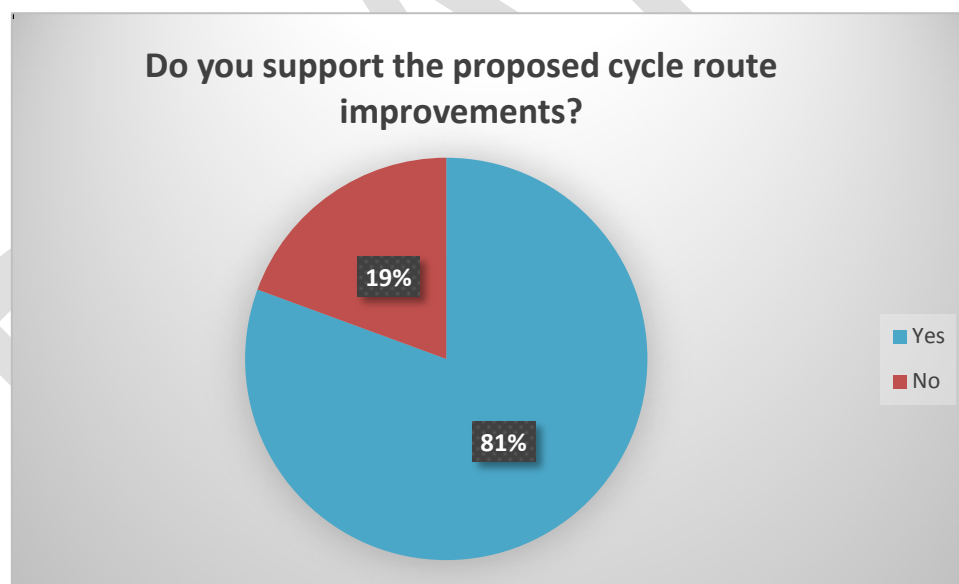


Figure 2: Pie chart showing the results of supporting or disagreeing with the proposed cycle route improvement scheme

Question 2 - Are there any alternative routes that should be considered?

The responses to question 2 can be seen in Figure 3. The responses for this question were mixed, with 52% of respondents answering 'no' suggesting that all routes had been included within the proposed design. However, 48% respondents answered 'yes' suggesting that there were more routes to be considered.

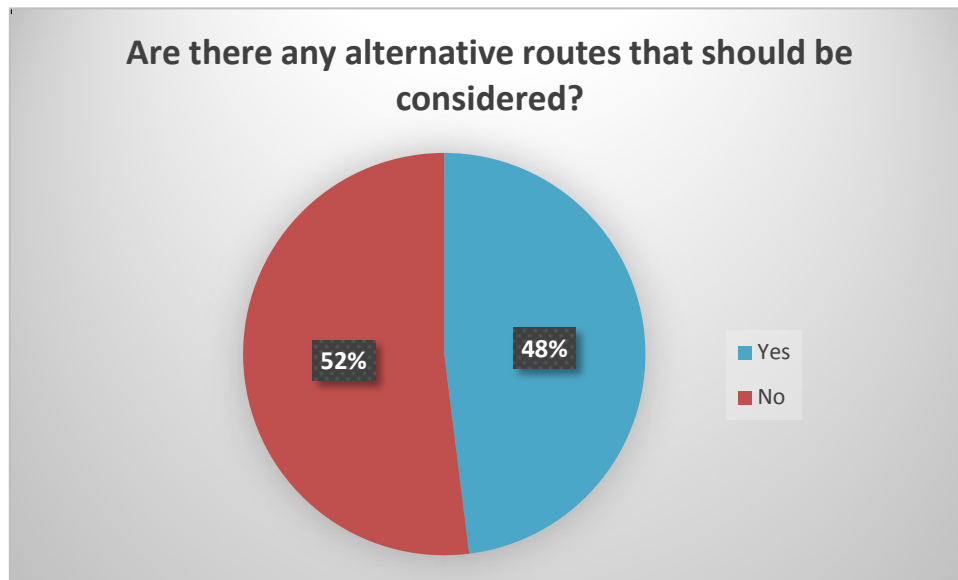


Figure 3: Pie chart depicting the results from Question 2 showing if the respondents thought there were more alternative routes to consider

Question 3 – There are seven route sections identified for improvement on the proposal plans, which route are you likely to use the most?

The respondents answered this question using a ranking system between 1 and 7. The route that the respondents were most likely to use was ranked 1 and the route that the respondent were least likely to use was ranked 7. Figure 4 shows the results for the route the respondents ranked as the number 1 route they were most likely to use.

The Mersey – Sale Water Park (routes 02 and 03) was the route ranked 1 out of 7 by the most respondents with 45%. The next route most commonly ranked 1 by respondents was significantly less with 14% answering for the Mersey-Bridgewater Canal (route 04). The Bridgewater Canal Connection (route 05), Mersey – Carrington Spur (route 06) and the Urmston Link (route 7) were ranked equally as the most used route with 10% of respondents ranking each of them number 1. The Dane Road Access (route 01) was ranked 1 similarly by 9% of the respondents. The route ranked 1 out of 7 by the least number of respondents was the South East Access (route 08) with 2% voting for it as their most likely route to use.

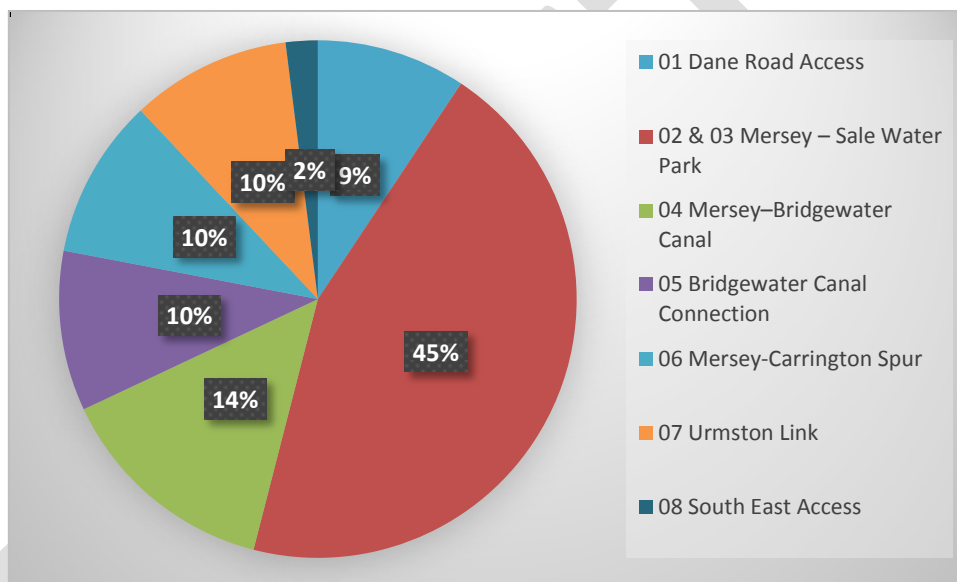


Figure 4: Pie chart showing the results respondents for first preference route of use between Urmston and Sale Water Park

The route that the respondents were second most likely to use was ranked 2. Figure 5 shows the results for the route the respondents ranked as the number 2 route they were most likely to use. The Mersey – Bridgewater Canal (routes 04) was the route ranked 2 out of 7 by the most respondents with 29%. The next route most commonly ranked 2 by respondents was similar with 24% answering for the Mersey-Sale Water Canal (route 02 and 03). The Bridgewater Canal Connection (route 05) and Mersey – Carrington Spur (route 06) were ranked equally as the second most used route with 13% of respondents ranking each of them number 2 out of 7. The Dane Road Access (route 01) was ranked the second most used route by 10% of the respondents whereas the Urmston Link (route 07) was ranked the second most used route by 8% of respondents. The route ranked 2 out of 7 by the least number of respondents was the South East Access (route 08) with 3%.

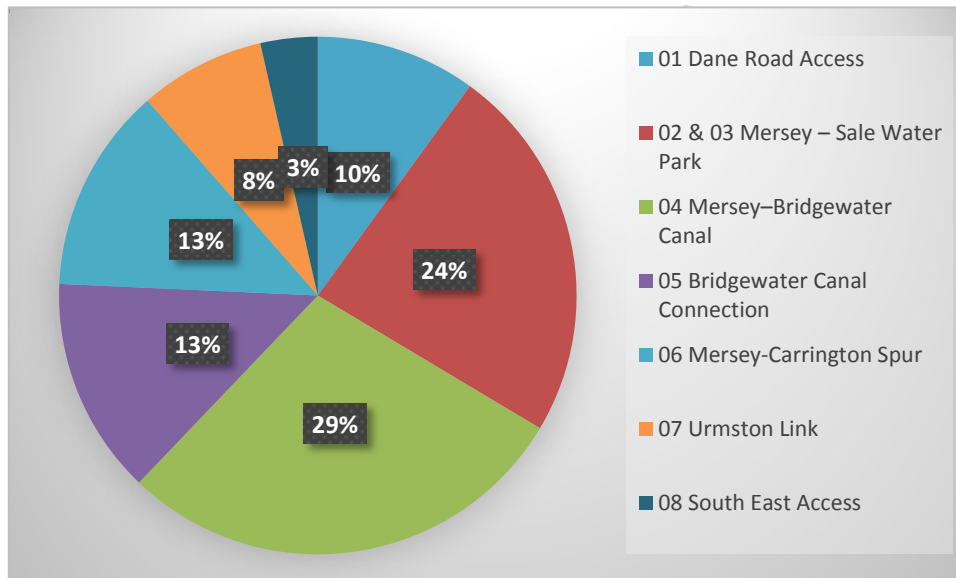


Figure 5: Pie chart showing the results for respondents' second preference route of use between Urmston and Sale Water Park

The route that the respondents were third most likely to use was ranked 3 out of 7. Figure 6 shows the results for the route the respondents ranked as the number 3 route they were most likely to use. The Bridgewater Canal Connection (route 05) was the route ranked 3 out of 7 by the most respondents with 29%. The next route most commonly ranked 3 by respondents was similar with 22% answering for the Mersey-Bridgewater Canal (route 04), 13% and 12% of respondents suggested that Mersey – Sale Water Park (route 02 and 03) and Mersey – Carrington Spur (route 06) respectively as their third most used route. The Dane Road Access (route 01) and South East Access (route 08) were ranked equally as the third most used route with 9% of respondents ranking each of them number 3 out of 7. The route ranked 3 out of 7 by the least number of respondents was the Urmston Link (route 07) with 6%.

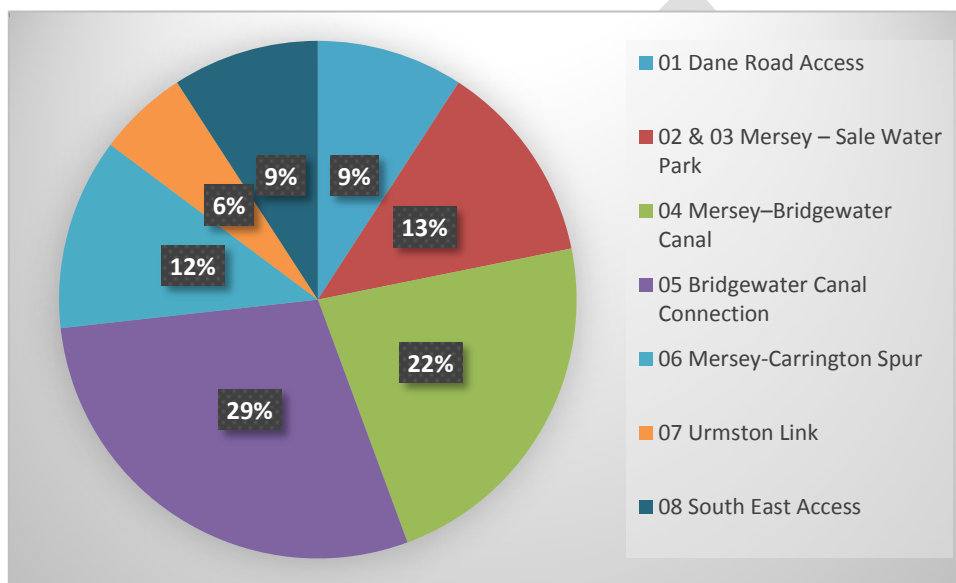


Figure 6: Pie chart showing the results for respondents' third preference route of use between Urmston and Sale Water Park

The route that the respondents were fourth most likely to use was ranked 4 out of 7. Figure 7 shows the results for the route the respondents ranked as the number 4 route they were most likely to use. The Dane Road Access (route 01) was the route ranked 4 out of 7 by the most respondents with 23%. The next route most commonly ranked 4 by respondents was similar with 20% answering for the Bridgewater Canal Connection (route 05). The Mersey – Carrington Spur (route 06) and the Mersey – Bridgewater Canal (route 04) were ranked equally as the fourth most used route with 13% of respondents ranking each of them number 4 out of 7. 12% and 10% of respondents suggested that Urmston Link (route 07) and Mersey – Sale Water park (route 02 and 03) respectively as their fourth most used route. The route ranked 4 out of 7 by the least number of respondents was the South East Access (route 08) with 9%.

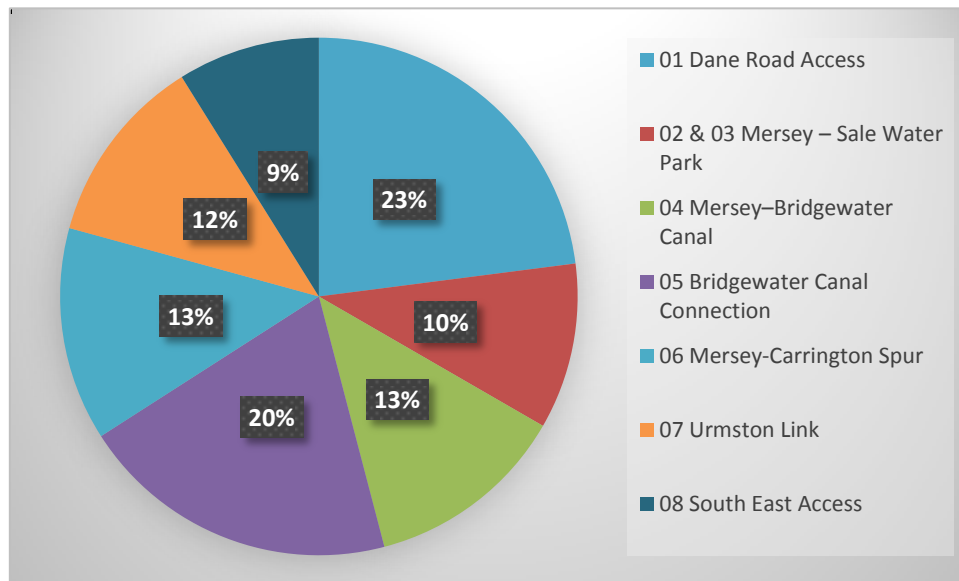


Figure 7: Pie chart showing the results for respondents' fourth preference route of use between Urmston and Sale Water Park

The route that the respondents were fifth most likely to use was ranked 5 out of 7. Figure 8 shows the results for the route the respondents ranked as the number 5 route they were most likely to use. The Mersey – Carrington Spur (route 06) was the route ranked 5 out of 7 by the most respondents with 21%. The next route most commonly ranked 5 by respondents was similar with 20% answering for the Urmston Link (route 07). The Dane Road Access (route 01) was ranked 5 of 7 by 18% of respondents. 15%, 11% and 10% of respondents suggested that South East Access (route 08), the Bridgewater Canal Connection (route 05) and Mersey – Bridgewater Canal (route 04) as their fifth most used route respectively. The route ranked 5 out of 7 by the least number of respondents was the Mersey – Sale Water Park (route 02 and 03) with 5%.

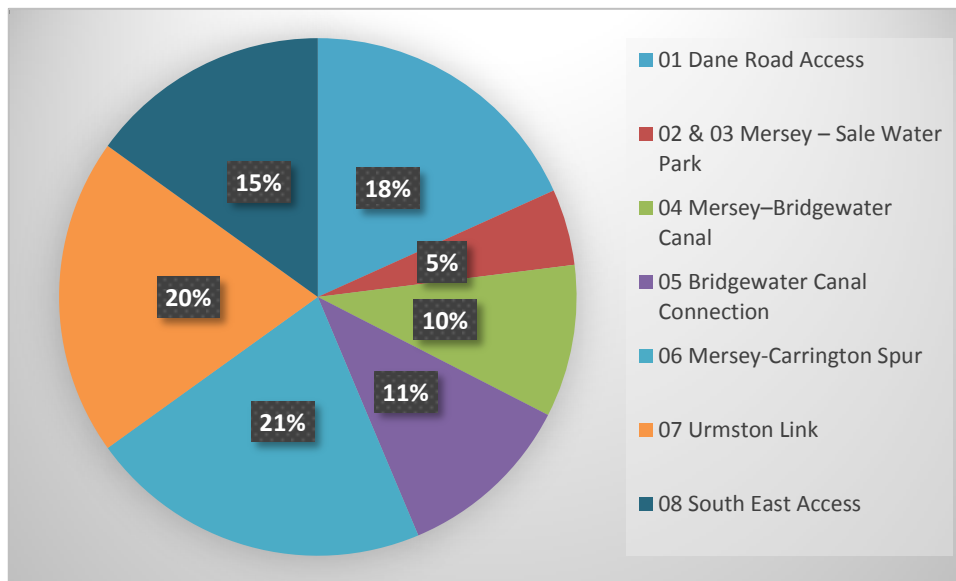


Figure 8: Pie chart showing the results for respondents' fifth preference route of use between Urmston and Sale Water Park

The route that the respondents were sixth most likely to use was ranked 6 out of 7. Figure 9 shows the results for the route the respondents ranked as the number 6 route they were most likely to use. The Urmston Link (route 07) was the route ranked 6 out of 7 by the most respondents with 35%. The next route most commonly ranked 6 by respondents was significantly less with 21% answering for the South East Access (route 08). The Dane Road Access (route 01) was ranked 6 of 7 by 16% of respondents. The Mersey – Carrington Spur (route 06) and the Bridgewater Canal Connection (route 05) were ranked equally as the sixth most used route with 10% of respondents ranking each of them number 6 out of 7. The Mersey – Bridgewater Canal (route 04) was ranked 6 of 7 by 6% of respondents. The route ranked 6 out of 7 by the least number of respondents was the Mersey – Sale Water Park (route 02 and 03) with 2%.

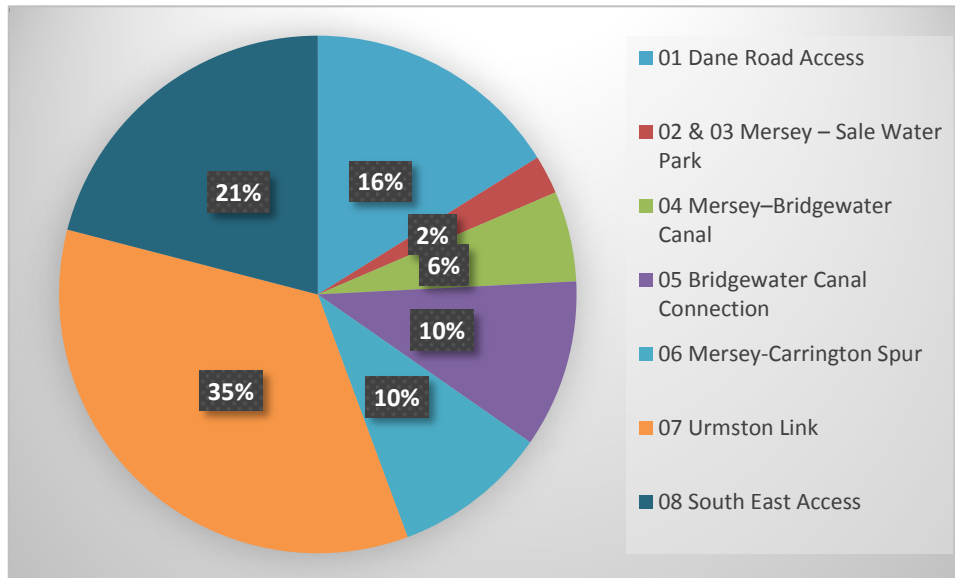


Figure 9: Pie chart showing the results for respondents sixth preference route of use between Urmston and Sale Water Park

The route that the respondents were least likely to use out of the seven available was ranked 7 out of 7. Figure 10 shows the results for the route the respondents ranked as the seventh route they were most likely to use. The South East Access (route 08) was the route ranked 7 out of 7 by the most respondents with 45%. The next route most commonly ranked 7 by respondents was significantly less with 18% answering for the Dane Road Access (route 01). The Mersey – Carrington Spur (route 06) was ranked 7 of 7 by 16% of respondents. 15% and 4% of respondents answered the Urmston Link (route 07) and the Bridgewater Canal Connection (route 05) respectively as their fifth most used route. The routes ranked 7 out of 7 equally by the least number of respondents was the Mersey – Sale Water Park (route 02 and 03) and Mersey – Bridgewater Canal (route 04) with 1% each.

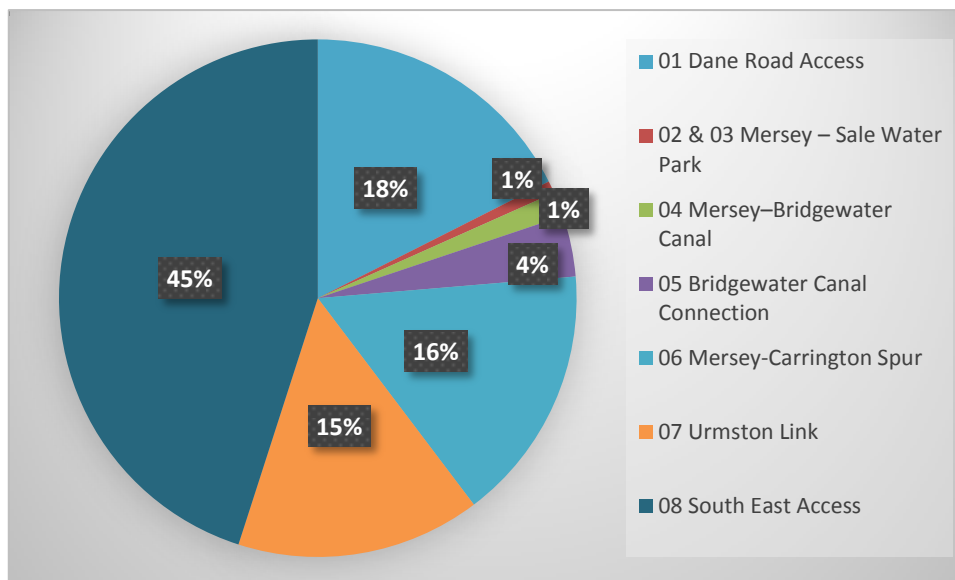
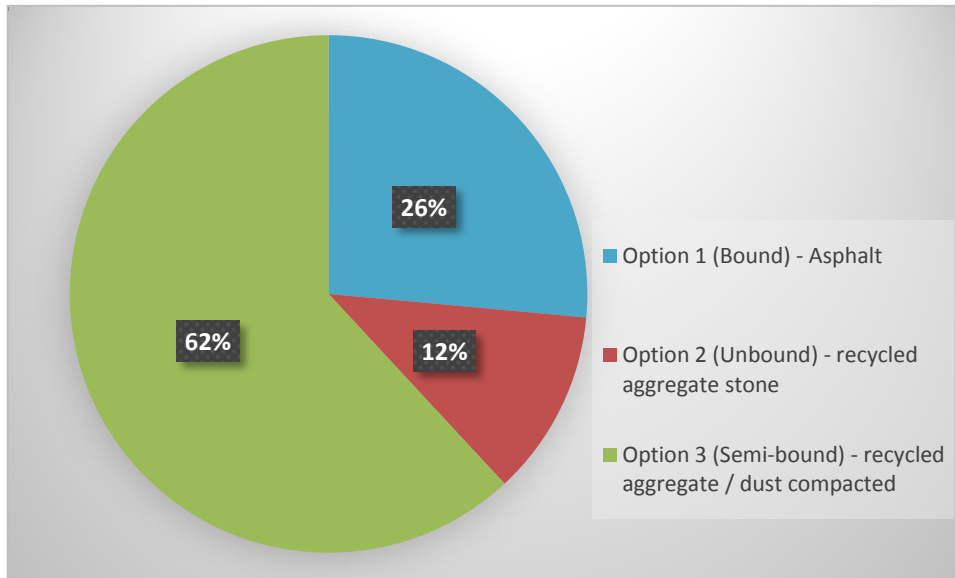


Figure 10: Pie chart showing the results for respondents seventh preference route of use between Urmston and Sale Water Park

Question 4 - Do you have a preferred surface treatment?

The responses for question 4 can be seen in Figure 11. The results show that the most preferred surface treatment option to be 'Option 3 (Semi-bound) – recycled aggregate' with 62% of the respondents voting for it. 26% of the respondents answered, 'Option 1 (Bound) – Asphalt' and the least preferred surface treatment option was 'Option 2 (Unbound) – recycled aggregate stone' with only 12% of respondents



choosing it.

Figure 11: Pie chart showing results for preferred surface treatment

Question 5 - Do you support the principle of providing a new bridge that will improve access for all users at Sale Water Park and the crossing at Jackson’s Boat?

The responses for question 5 can be seen in Figure 12. The results show that there is overwhelming support for a new bridge at Jackson’s Boat, that will improve access to all users at Sale Water Park and the Mersey river crossing at Jackson’s Boat. This verdict is due to 89% answering yes to the support. Contrasting that result, 11% of the respondents answered to not support a new bridge being provided.

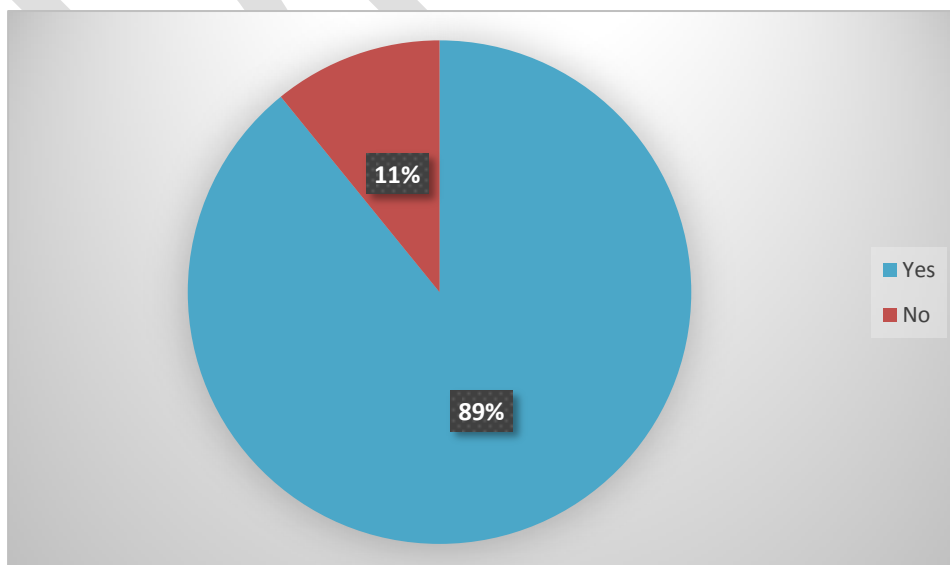


Figure 12: Pie chart showing results for level of support for new bridge at Jackson’s Boat

Question 6 - Which of the proposed new bridge options do you prefer?

The responses for question 6 can be seen in Figure 13. The results show there is a mixed opinion towards the proposed bridge options. 55% of the respondents preferred the Warren Truss Bridge where as 45% preferred the Butterfly Arch Bridge.

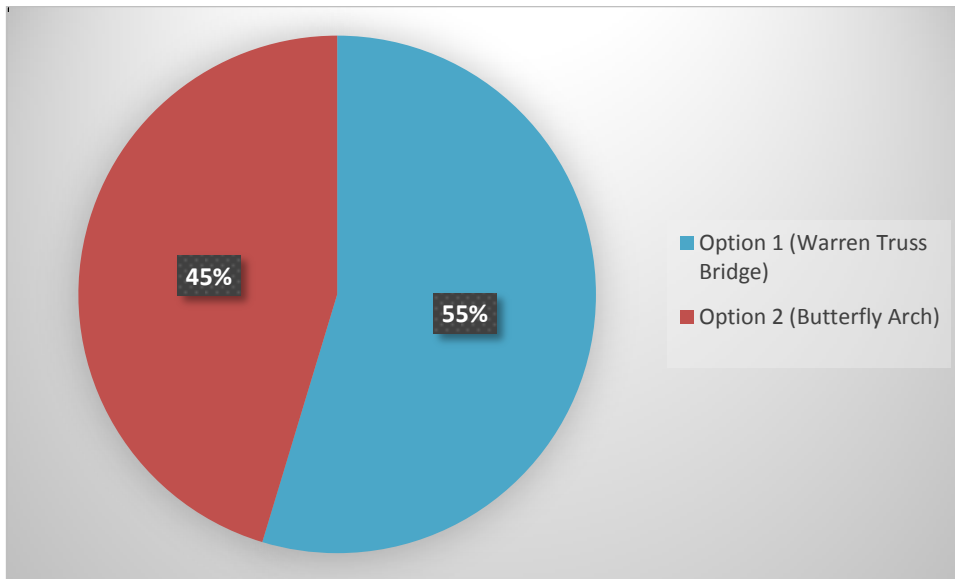


Figure 13: Pie chart showing the preference of proposed bridge option at Jackson's Boat

Question 7 -Do you support the proposed approximate location of the new bridge option?

The responses for question 7 can be seen in Figure 14. The results show that there is a large amount of support for the proposed approximate location of the new bridge at Jackson's Boat with 89% of respondents answering yes. Conversely, 11% of the respondents answered to not supporting the location of the proposed bridge.

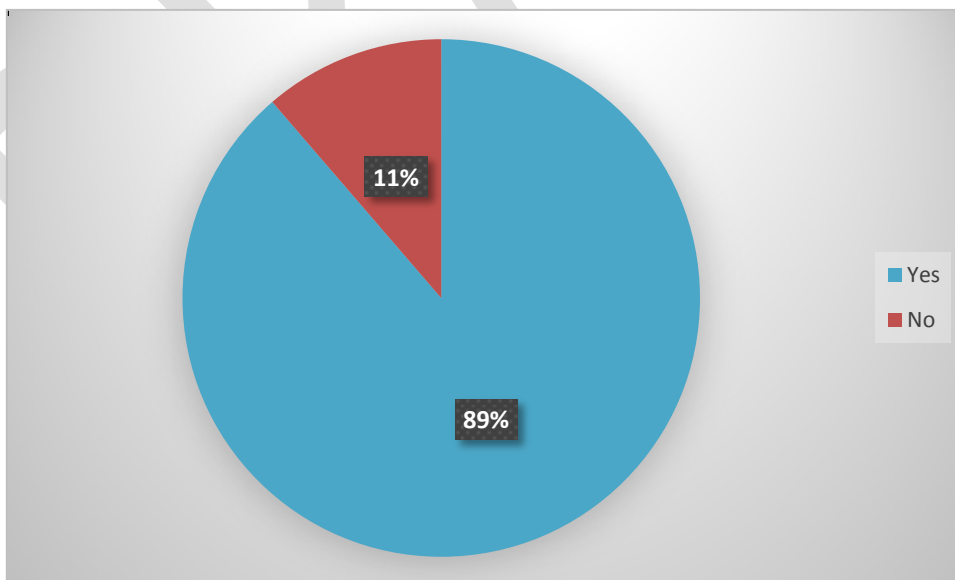


Figure 14: Pie chart showing level of support for the proposed approximate location of the new bridge at Jackson's Boat

2.3. Qualitative Analysis

Respondents had the opportunity to leave additional comments after questions 1 to 7 about the proposals as part of the online survey. The comments received varied in length, detail and nature. The qualitative analysis of these comments was broken down into three topics. These topics are:

- The overall cycle scheme improvements between Urmston and Sale Water Park;
- The Surface treatment option to be use; and
- The proposed Bridge at Jackson's Boat.

The key themes within each topic were revealed through a detailed analysis of the comments received by 232 respondents of the public consultation as well as 19 (see Appendix C, Table 8) additional emails received.

Cycle scheme improvements between Urmston and Sale Water Park

A major theme in the analysis of the overall cycle improvement scheme was the notable concern over the exclusion of equestrians. According to several respondents, there are more than 1000 horses that are rode in the Trafford Area. Many of respondents have negative feelings towards the proposed overall scheme due to the exclusion of equestrians.

There is overwhelming concern that the proposed improvement scheme gives priority to the needs of cyclists without consideration of equestrians. There is also concern that proposed improvements will encourage excessive speed of cyclists, which may result in the horses becoming intimidated.

Another major theme of the overall scheme was negative support towards the scheme due to public perception that the local Trafford Council funding should be spent in other areas. Respondents from the public consultation believed that public funding should be spent on improving road infrastructure as well as measures to decrease the rate of crime and anti-social behaviour in the Trafford region.

A highly recurring theme throughout the comments in relation to the routes being updated is a request for an additional cycle route to be created through Turn Moss Playing fields (approximately 150 meters long) linking Turn Moss car park/Turn Moss Road and Hawthorn Lane.

An additional recurring theme within the comments received was the concern of how the overall improvements of the routes for cyclists encourage excessive speed of cyclists which is to the detriment of pedestrian safety in a shared route environment. Many pedestrians objected to the shared use route due to fear of colliding with high speed cyclists.

As well as this, there are numerous mentions of 2-wheeled and 4-wheeled motorised vehicles using the existing routes illegally. There is objection to the removals of barriers as minority express this could cause an increase in their presences. A related theme in this respect are requests to remove barriers to ensure wheelchair users can use the routes

Another overwhelming theme found throughout the comments relating to the overall improvements of the scheme was the positive support of cyclists towards the gaining an off-road facility safe for cyclists of all abilities.

Surface Treatment to be used

The overwhelming theme for comments related to proposed surface treatment to be used was that it is to be suitable for equestrians, cyclists and pedestrians. Many comments show concern towards exclusion of horse riders.

Other recurring themes within the qualitative analysis is the ability for the surface treatment to be easily maintained, aesthetically pleasing and of a style to be in keeping with the surrounding environment. As well as this there was notable acknowledgements from respondents that they were inexperienced in surface treatment design.

An additional theme repeatedly mentioned was that asphalt is thought by many to be usable throughout the year and is not as susceptible to flooding as the other unbound options. However here are numerous mentions of how surface water on asphalt freezes with ease in colder temperatures, leading to slippery and unsafe conditions for all users of the shared footway network. According to the equestrian respondents, asphalt is not an appropriate surface treatment for bridleways due to the low friction provided by the surface and the metal horse shoe.

Another recurring theme related to the surface treatment topic was the importance of asphalt being used as the surface treatment material to aid wheelchair users.

Proposed Bridge at Jackson's Boat

A major theme found in the qualitative analysis relating to the proposed bridge design at Jackson's Boat was the recognition that the existing bridge is currently below current standards. This includes recognition of the narrow width, sub-standard decking and sub-standard disabled access from Jackson's Boat Car Park.

Other additional themes that dominated the comments relating to the proposed bridge at Jackson's Boat was concern that the design did not take equestrians into consideration as well as the opinion of choosing the cheapest option between the Warren Truss and Butterfly Arch Design.

Although not mentioned to the same extent as the details above, another recurring theme mentioned in the analysis was the discontent of the removal of the existing bridge, support of the new bridge while keeping the existing and concern of there being no crossing available during the proposed bridge construction. As well as this there was notable acknowledgements from respondents that they were inexperienced in bridge design.

Minor points mentioned by a small number of respondents was the requirement for the new bridge to be aesthetically pleasing and to match the style of the existing bridge as well as possible.

3. Discussion

Upon reviewing the quantitative and qualitative analysis the discussion can be split into three main sections. These sections are:

- The overall cycle scheme improvements between Urmston and Sale Water Park;
- The Surface treatment option to be used; and
- The proposed bridge at Jackson's Boat.

3.1. Overall cycle scheme improvements between Urmston and Sale Water Park

After reviewing the results in the quantitative analysis there appears to be a large majority of support for the proposed cycle route improvements as 81% of the respondents to the multiple-choice survey answered yes to question one (see Figure 2). However, upon reviewing the additional comments, a major theme that recurred in the qualitative analysis was that support was based upon the proposed improvements taking all users into consideration i.e. Pedestrians, cyclists, horse riders and wheelchair users.

The information displayed in Figures 4 to 10 shows which routes are the most popularly used. Figure 4 shows that 45% of respondents choose 'Route 02 & 03 Mersey to Sale Water Park' and 2% of respondents choose 'Route 08 South East Access' as their first preference route of use. Following that trend, Figure 10 shows 1% of respondents choose 'Route 02 & 03 Mersey to Sale Water Park' and 45% of respondents choose 'Route 08 South East Access' as their last preference route of use.

From the information displayed in Figures 4 to 10, it can be determined that routes can be ranked by preference of use in the following order:

- 1) 02 & 03 – Mersey to Sale Water Park
- 2) 04 – Mersey to Bridgewater Canal
- 3) 05 – Bridgewater Canal Connection
- 4) 01 – Dale Road Access
- 5) 06 – Mersey to Carrington Spur
- 6) 07 – Urmston Link
- 7) 08 - South East Access

The theme that dominated the qualitative analysis was the concern of equestrian exclusion. Over 100 of the 232 respondents drew attention to the needs of horse riders. Numerous respondents object to bridleways becoming paved shared cycle paths, removing safe and off-road facilities for horses which are already a limited commodity in the area.

Several respondents expressed that "there are over 1000 horses in the local area (more in the whole of Trafford) and there is good usage of the paths/tracks/bridleways". However, information on the period for this horse population was not obtained. Although, it is clear from the results that the needs of equestrians must be considered in equal measure to cyclist and pedestrians.

From the qualitative analysis there is much concern over an additional cycle route through Turn Moss Playing fields not being considered as part of the cycle improvement scheme. This a short stretch of approximately 150m in great need of linking Turn Moss car park/Turn Moss Road and Hawthorn Lane. This is thought by many (see Appendix B) to improve the National Cycle Route No.62 with existing cycle routes through the surrounding area.

Another major theme was negative support towards the scheme due to public perception that the local Trafford Council funding should be spent in other areas. Numerous respondents from the public consultation

believed that public funding should be spent on improving road infrastructure as well as measures to decrease the rate of crime and anti-social behaviour in the Trafford region.

An additional recurring theme within the comments received was the concern of how the overall improvements of the cycle scheme encourage excessive speed of cyclists and the direct risk increase to the safety of pedestrians in a shared footway environment. Many pedestrians are in objection to the shared footway system due to fear of colliding with high speed cyclists.

As well as this there is concern over the presence of motorised scooters and quad-bikes within the existing shared footway system. There is reasonable public concern that the proposed removal of barriers will encourage more motorised vehicles to use the system putting the safety of all other users at risk.

3.2. Surface treatment option

The quantitative analysis shows that the majority of respondents are in favour of the 'Option 3 (semi – bound) – recycled aggregate / dust compacted' as 62% of respondents answered in favour of it when compared to 'Option 1 (Bound) – Asphalt' and 'Option 2 (Unbound) – recycled aggregate stone'.

Having viewed the qualitative analysis the overwhelming theme for comments related to proposed surface treatment to be used was that it must be suitable for horses, cyclists and pedestrians. Many comments show concern towards exclusion of equestrians. According to the equestrian respondents, asphalt is not an appropriate surface treatment for bridleways due to the low friction provided by the surface and the metal horse shoe. Asphalt therefore potentially puts horse riders and other users' safety at risk. This opinion has perhaps led to 62% voting in favour of option 3, however a small number of respondents suggested the use of 'nu-phalt' as a more appropriate surface treatment for horses.

Due to recurring themes in the qualitative analysis it seems the support for 'Option 3' is rooted in its more natural aesthetically pleasing look when compared to asphalt. However, with 26% of respondents answering in favour of 'Option 1 (Bound) – Asphalt' in the online survey there is reasonable support for this surface treatment to be used. Although it is not a dominating theme in the qualitative analysis, there is a notable theme of support towards use of asphalt surfacing.

After reviewing the additional comments for question 4 in details, the reasoning behind this is due to the easy maintenance nature of asphalt. Many respondents support the use of asphalt due to it being the smoothest surface available, enabling easier movement for road bikes and wheelchairs. However, this conflicts with the concern of pedestrians that asphalt paving in a shared footway environment encourages excessive speed of cyclists.

It should also be highlighted that a common theme in the qualitative analysis was the respondents' acknowledgement of their inexperience in surface treatment design causing many to refrain from commenting. However, from the research conducted throughout the consultation and the options available, 'option 3 (semi – bound) – recycled aggregate / dust compacted' would appear to be the logical compromise suitable for horses, cyclists and pedestrians.

3.3. Proposed Bridge at Jackson's Boat

A major theme found in the qualitative analysis relating to the proposed bridge design at Jackson's Boat was the recognition that the existing bridge is currently below existing standards. This was due to the existing narrow width, sub-standard decking and unacceptable disabled access from the Jackson's Boat Car Park. This collective opinion is supported by the results from question 5 of the multiple-choice questionnaire as 89% of the respondents answered in favour of a new bridge at Jackson's Boat that will improve access for all users (see Figure 12).

This contrasts with the 11% of respondents that answered in objection to a replacement bridge. The additional comments to question 5 suggests that the 11% of respondents are fond of the character the bridge gives the area due to its age and traditional look. It was also determined from analysing the comments that although there is general recognition of the need for a new bridge, numerous respondents show fondness towards the existing bridge and suggest it is kept for another purpose.

With this considered, the dominating theme in the comments in relation to a new bridge proposal is the concern that equestrian have not been considered as part of the design. An overwhelming amount of comments highlight that the proposed bridge design only considers the needs and standards of cyclists and pedestrians but not equestrians.

Another recurring theme that was notable in the qualitative analysis was the concern that there would be no crossing available at Jackson's Boat during the construction on a new bridge. This is a null point as the existing bridge will remain accessible during any potential construction period should a proposed bridge option be selected. Due to the existing bridge achieving Grade II listed status (as of 25th January 2019), it will subsequently remain in place for all proposed construction scenarios.

Question 6 of the quantitative analysis displays a mixed opinion towards the two proposed bridge options that were available for selection. 45% and 55% of respondents answered in favour of the butterfly arch and warren truss bridge respectively (see Figure 13). Upon reviewing the additional comments for question 6 (see appendix B) support for the warren truss is due to its style being more in keeping with the existing bridge and the support for the butterfly arch is due to its grand aesthetics. Albeit a missed result from the multiple-choice survey, a major theme found in the comments was the respondents' acknowledgement of their inexperience in bridge design and gave comment that they would prefer the option that had the lesser economic cost. It should be noted that the final design will be influenced by the planning process especially in relation to the now Grade 2 listed existing bridge.

Question 7 of the quantitative analysis shows that there is a large amount of support for the proposed approximate location of the new bridge at Jackson's Boat with 89% of respondents answering yes (see Figure 14). Conversely, 11% of the respondents answered to not supporting the location of the proposed bridge. These results show an overwhelming support from the respondents of the public consultation toward the proposed location of the new bridge option at Jackson's Boat. Therefore, from the research conducted throughout the consultation and the options available a new warren truss bridge at Jackson's Boat is generally the favoured option. This is under the condition that the existing bridge will remain accessible.

4. Conclusion

4.1. Main Findings

Overall the public's response to the proposals was positive, with over 81% of respondents in support of the proposed cycle route improvements. Despite strong support of the proposals, there were a number of negative responses received throughout the public consultation process which highlighted several key points or concerns which should be reviewed and addressed where possible at detailed design stage. These key findings are listed below:

- Inclusion of equestrians and horses must be taken into consideration during the detailed design process. This will include the facilitation of their needs within the existing bridleways, the surface treatment option and the proposed bridge design;
- Very positive response about the routes included in the improvement scheme. However, cycle route should be considered through Turn Moss playing fields. This a short stretch of approximately 150m in great need of linking Turn Moss car park/Turn Moss Road and Hawthorn Lane;
- Pedestrians are in objection to the shared footway system due to fear of colliding with high speed cyclists. The asphalt paved surface may encourage excessive speed of cyclists;
- Public concern that the proposed removal of barriers will encourage more motorised vehicles to use the system putting the safety of all other users at risk;
- Asphalt is not an appropriate surface treatment for bridleways due to the low friction provided by the surface and the metal horse shoe. Options 3 (semi – bound) – recycled aggregate / dust compacted' would appear to be the logical compromise suitable for horses, cyclists and pedestrians
- General recognition that the existing bridge structure at Jackson's Boat is below suitable standards;
- Concern that the two proposed bridge options located at Jackson's Bridge presented in the public consultation only meet the needs of pedestrians and cyclists. Inclusion of equestrians is needed; and
- A warren truss bridge option is preferred to the butterfly arch due to its aesthetic style being more in keeping with the existing surroundings. However, the views of planners (including heritage officers) will probably determine the type of bridge to progress, i.e. a warren truss or butterfly arch taking into account the Grade 2 listed status of the existing bridge.
- Due to the existing bridge achieving Grade II listed status (as of 25th January 2019), it is to remain in place for all proposed construction scenarios.

4.2. Action Required

Based on the public consultation undertaken to date, there is strong support for the proposals involved in the Urmston to Sale Water Park Cycle Improvement scheme, however a number of concerns have been raised by local groups/user which must be carefully considered throughout the detailed design stage.

Following the sensitive analysis of comments all routes suggested in the feasibility design (including Jackson's boat bridge), the appropriate financial backing can be given with the dedicated resource from Highways England and local councils.

Based on the analysis of the consultation results it is recommended that the unbound surface treatment is used for the proposed routes to cater for all users including, equestrians, cyclists and pedestrians. This is with the exception of routes alongside River Mersey to Sale Water Park (route 02 and 03) and Carrington Spur (route 06). This is due to the location of these proposed routes along the top section of the embankments where there is varying available width and equestrians can utilise the wider paths alongside the river bed. It is therefore proposed to provide bound shared paths along the top of the embankments for the provision of cyclists and pedestrians only.

In addition, it is also recommended that a cycle route be considered for design through Turn Moss playing fields and the proposed warren truss bridge be constructed in a location subject to agreement with Trafford Council and Local Planning Authority. The existing bridge should remain for all proposed construction scenarios. Amendments for the proposed bridge design is required to cater for equestrians as well as cyclists and pedestrians. The scheme however is to be developed and progressed within the constraints of the budgets available via the grant.

DRAFT

Appendix A: Sample Questionnaire

Proposed Urmston to Sale Water Park Cycle Route Improvements Scheme

Q.1 Do you support the proposed cycle route improvements?

Yes

No

Please give any other comments you have below: -

.....
.....
.....

Q.2 Are there any alternative routes that should be considered?

Yes

No

Please give any other comments you have below: -

.....
.....
.....

Q.3 There are seven route sections identified for improvement on the proposal plans, which route are you likely to use most?

(please rank 1 (most)- 7 (least))

01-Dane Road Access

02 & 03- Mersey – Sale Water Park

04-Mersey–Bridgewater Canal

05-Bridgewater Canal Connection

06- Mersey-Carrington Spur

07- Urmston Link

08-South East Access

Q.4 Do you have a preferred surface treatment?

(please select one only)

- Option 1 (Bound) - Asphalt
- Option 2 (Unbound) - recycled aggregate stone
- Option 3 (Semi-bound) - recycled aggregate / dust compacted

Please give any other comments you have below: -

.....

.....

.....

.....

Q.5 Do you support the principle of providing a new bridge that will improved access for all users of Sale Water Park and the crossing at Jacksons Boat?

(please select one only)

- Yes No

Please give any other comments you have below: -

.....

.....

.....

.....

Q.6 Which of the proposed new bridge options do you prefer?

(please select one only)

- Option 1 (Warren Truss Bridge)
- Option 2 (Butterfly Arch)

Please give any other comments you have below: -

.....

.....

.....

.....

Appendix B: Comments from Public Consolation

(NOTE: All comments reported with the grammar, spelling and format in which they were received)

It should be noted that all comments documented in Appendix B were received prior to 25th January 2019 when the existing bridge located at Jackson's Boat was added to the List of Buildings of Special Architectural or Historic Interest by the Secretary of State for Digital, Culture, Media and Sport. The bridge is listed at Grade II.

Table 1: Additional Comments given in relation to Question 1 of the online multiple-choice questionnaire

I am a new cyclist and appreciate safe off road cycling to build my confidence
Yes, as a Stretford resident who tries to cycle with children, this would revive our car use.
Bound gravel over black top.
Really important that cycling access across the whole GM area is improved. Access through from Urmston and Sale via the current route is feasible but sub-optima I support proposals to improve this
I run a social cycling Group in Urmston (Urmston Social Cycling - it's a Facebook Group) which was only established earlier this year and now has over 600 members. My ambition in establishing the Group was to simply encourage people out on their bikes. It's been a tremendous success so far. Many of the 130+ individuals who've joined rides did not cycle at all previously. TfGM are currently delivering 'Road Rider Ready' courses in Urmston specifically for the Group due to its popularity. Due to the nature of the Group the routes that I advocate are a mixture of on and off road surfaces and the proposed improvements here are of tremendous interest to me. My main observation concerning the proposals relate to the choice of entry to the route from Urmston. Loretto Road (your proposed route) is not the most obvious choice for those hoping to join the route from Urmston, albeit that any improvements that can be made to that route would be welcome - see comments below.
I would rather money was spent improving road routes, this is already an easy route to ride
Trafford needs more cycling infrastructure and as a keen cyclist i'd like my not so keen cyclist family to come with me and not worry about traffic
Horse riders should have access to the route
I am a horse rider not a cyclist but am assuming we are included as users of these routes? I have been riding in this area for the past 40 years and seen changes but always with the cyclist in mind I like to think horse riders are just as important!
The worst section linking Urmston with the improvement area along the Mersey running under the Carrington spur road has not been mentioned. The connection from here towards Meadow road and also towards Eesbrook Lane (to connect with Urmston Meadows) is a mud bath most of the time and is far worse than many of the paths with proposed improvements. Surely money would be better spent making these paths useable.
I've known the Mersey Valley for around 45 years now and for all of that time it has been a bit muddy and even a bit inaccessible. These characteristics have been part of the joy of it for me. Now you are planning to tidy it up and 'civilise' it. People who want access to the countryside should be prepared to put up with a bit of mud!
There are other things that local councils should be spending money on like crime and anti social behsviour. This needs to be tackled first. There is no way I would cycle or let my child cycle when it's not safe to do so. Look at all the attacks on the Fallowfield Loop.
There is no need to put full footpath construction on the majority of these areas (by which I assume you mean concrete or tarmac, as it's not spelled out) . The existing trail footpath is perfectly adequate for both bikes and people. In some areas which can get a bit boggy there is room for improvement (eg connection towards Urmston from the Bridgewater) but for the vast majority of areas the existing infrastructure is adequate, even after rainy periods. I am both a cyclist and walker/runner in good weather and bad. Ironically you don't seem to have included the one section that is completely not fit for purpose. I'll also further comment that if you tarmac this then you will run the risk of the conflicts that are occurring on the Bridgewater canal since it's upgrade. It did need improvements but now it's time trial cyclists vs the slow moving pedestrian/animals with the many disputes that have followed. As a final note

I do support improvements to the Dane road access section as it's not good.
Long overdue
I do not use this for cycling but do support it. However please make sure it remains suitable for horses as many and many others frequently use this and need a suitable surface
But I do question why horse riders have been excluded from the scheme, riders are as vulnerable on the roads as cyclists and pedestrians
I am a horse rider but we do not seem to have a say in this
I have concerns regarding the effect these new routes may have to bridle pathways and access routes for horse & riders which don't appear to be considered.
Hi I support the cycle improvements however they have to be horse and walker friendly also. There have been numerous occasions when cycles come too fast around corners causing near collisions.
Equestrian users should also be considered. This is a popular pastime and a healthy way to enjoy this route
Should be improvements for all users including horse riding
They do not take into account of the current bridle path status. Drivers want horseriders off roads and changing the status of bridle paths means there are less and less safe places to ride (and more and more horseriders on the roads).
As a horsewoman and cyclist. I should hope both sports will be taken into account whilst improvements are being planned and put into action. I have used the routes previously whilst on horseback and wish to continue to do so.
Provided that access is given to horse riders on the appropriate tracks
Need to improve biodiversity along the route
where are the bridleways going it's not only walkers cyclist that use these routes, in fact cyclist are a problem for walkers
But not destroying Jackson's Boat Bridge
These paths are used by horse riders. Tarmac-ing them means it makes them slippery for horses and we all know that cyclists (not all but some) don't show any regards for horses or walkers for that matter.
Although I am not averse to improving cycle routes your proposal has not taken into account or even made mention to the equine connection on the proposed route. There are at least two bridleway sections on the proposed route which are well used by horses and riders and equestrian groups. You have made no mention of the measures you will put in place to safeguard the use of these bridleways for their intended users, namely horses.
Please can you confirm that the proposed changes will be appropriate for equestrian use
Until the bridleways are kept or mention of including horses is added I do not support this
I would use the riverside path a lot more if the surface was better. I will currently ride it on a mountain bike on my own, but would not ride it on a hybrid or road bike or with my young daughter on her bike seat. I also regularly attend Sale Water parkrun with an off road running buggy and the current route is only just passable. It would open up this route to many more people with buggies if the surface was improved.
I support it with reservations - see below re Jackson's Bridge.
At Barfoot bridge please extend the reconstruction of the path for just a few 100 metres along the river on the Stretford side, to meet the steps access to Hawthorn Road towards the Chorlton brook, as this flood all winter long. There is a broken gate and styal at this location.
great
Yes
Yes, asking as they also encourage multi user access. I.e better access for prams, wheel chairs, horse riders
As you have not mentioned equestrian use. Please consider this!
Yes assuming they continue to allow access and safe off road hacking for horseriders

However, this should be a multi user route to also include horse riders, wheeled chair users and families with push chairs
Please take into account horse riders using this route to.
Absolute need for this route to include and accommodate horse riders.
Please ensure that the improvements ensure accessibility for horses and riders.
Should also accommodate horse riders
Need safe provision for horses and riders
you propose to use BRIDLEWAYS but fail to mention accommodating horse riders.
Horse riders need including in the plans
Please remember this route is also used by many horse riders. We need to be taken into consideration also
Yes if provision made for horse riders too.
I have lived in Chorlton all my adult life . I am totally against the new bridge replacement at Jackson's Boat. Improvements of cycle routes and the trans pennines route could be initiated without the demolition of what could be a bridge with Heritage Status.
But should include RoW for horses
Only if access for horse riders is included along the route.
But please consider horse riders
I only support them if they include horse riders
Surely a bridleway cannot be taken away and made into a cycle track. Bridleways have a purpose and are used as such for the purpose they are intended for.
As it currently stands there is no mention of horse access. I am a horse rider and do use these areas... already areas I use around Elton reservoir in bury have had their bridle ways tarmaced and been partially converted into cycleways and now to have them used regularly by cars as they've been made too wide. We need to be consulted please!?
I would if promised that this would not effect access to horseriders to the bridleways.
I can't find any reference to ridden horses except ".Will equestrian users be able to use any proposed bridge at Jackson's Boat? The current bridge crossing is not defined as an equestrian route and it is not proposed to change the route designation as part of these proposals. The plan is unreadable and th photos show tarmac which is unsuitable for horses as it is both slippery and too hard; horses should not do more than five minutes a day trotting on tarmac.
Please ensure that horses / horse riders are included in the plans
But does this include horse riders too. Definitive map bridleways are inclusive to cyclists so surely where cyclists are considered for new routes so should horse riders.
I am a horse rider. The route needs to incorporate bridleways too.
There are many horse riders in this area using the existing tracks on which this route will be based and some of these are tracks are bridleways. Therefore, the route should also include horse riders. In addition, the route will run through a predominantly urban area and safe off-road routes for horse riding are essential.
It is imperative that the improvements take into account horse riders who regularly use these areas and need safe off road hacking
If improving the cycle way route affects the use as a bridle way for horses then no I don't support it.
As long as is it suitable for horse riders

I like the idea but you have missed equestrians who before hand have frequently used these paths
Please don't forget to include horse riders in your scheme. Many of us travel to use safe off road routes for riding our horses as well as our bikes.
I'm a horse rider and these proposals cut across two bridleways but don't appear to incorporate the needs of horse riders in the plans.
You need to consider horse riders using these routes as some of them are bridleway
I support the maintainability of all routes but please make it accessible to horses also.
Only if horse riders are considered!
We ride sections of this route that are currently bridleways, as current users we would like to be included in the scheme to ensure that our needs are met and we are not unable to use the paths once the upgrade has been done.
As local horse riders we feel we must be included so we can have access to off road routes.
Providing access for horse and riders still accessible.
On condition any improvements are also suitable and inclusive of horse riders.
What about continued access for horses? Forcing them off safe, pre-existing bridleways makes for greater risks on roads and reduces choice and access for a significant population of route users.
Please please include horse riders and make these routes multi-user paths! I
Where ever possible improvements should be all inclusive accommodating disabled access and horse riders and provide links to existing bridleways and the trans-pennine trail
what about horses???
Which is great support for cycling, but what about horse riders, more horses and their riders are killed on the roads than cyclists, yet their needs are not met.
Yes if it's for all users especially horse riders
Please ensure it is also suitable for horse riders.
Yes I would support improvements to the route PROVIDING it is open to walkers riders and cyclists. A safe nmu route is of more benefit and is more in line with the TPT in other districts.
The proposals will impact on the safety & well-being on pedestrians on the Bridgewater canal tow path. Cyclists are speeding along the tow path, intimidating & dominating the "shared" path . The proposals to remove the barriers which were designed to reduce speed of cyclists is fool hardy & will increase the danger to people enjoying a stroll.
I do support cycle lanes , on the main roads , but this is putting speeding cyclists , into quiet leisure areas.... not appropriate. I to am a cyclist, but find the Bridgewater canal towpath very dangerous, and have almost been knocked of my bike by inconsiderate cyclists speeding along, cutting me up, and generally being intimidating. A lot do not use their bells to let you know they are coming up. The barriers to slow cyclists down are inadequate and I have actually seen a cyclist , flick through the one at Springfield school and he hardly slowed down. We need more barriers, that actually work, not less, ones where the cyclists do not need to get off their bikes, but have to slalom through an obstacle, slowly. I would actually like to see the fast cyclists separated form the towpath completely. possibly a fast cycle lane following the Chester road/ Washway road. At the end of the day , the Bridge water canal towpath along with the Mersey valley are places people visit to get away from speeding cars and cyclists. These areas for casual strolling and fishing, feeding the birds, family walks, dog walking and leisure cycling, horse riding. And a cycle race track..... the two pursuits are incompatible.
The reason for my objection is that the creation of this proposed cycle route does not include equestrians and as such it will only further serve to fragment the bridleway network and force the most vulnerable users - horses - on to the roads which will increase the opportunity for accidents with vehicles. The route

<p>should be a bridleway available to equestrians. The only other alternative would be for it to be designated as an NMU - non motorised user route.</p>
<p>But existing rights of way, such as bridle paths should not be adversely affected.</p>
<p>But not at the expense of horse riders. Horses need safe, off road routes just as much as walkers & cyclists. You have not included horse riders in your proposed scheme.</p>
<p>Any proposals need to take into account all users and not just cyclists, I am a regular horse rider and use these routes all and cannot see how the bridle ways will be maintained for horses and how horse riders are being considered.</p>
<p>Being both a cyclist and an equestrian (occasional pedestrian) I am keen to support any project to improve access for these groups of users. But they must, as indicated, be accessible to all and I sometime feel the equestrians are sidelined when Cyclist take over existing bridleways.</p>
<p>Great idea, but should also include horse riders to get them off the roads</p>
<p>But you need to consider horse riders too</p>
<p>This route is used by horse riders. Please ensure they are taken account of when completing plans and allowed continued access and availability</p>
<p>I have concerns over the possible surface treatments, where the requirements of different users may conflict. In particular I have concerns that steps should be taken to deter high-speed cycling, and that the surfaces should be safe for horse riders. Removal of the barriers may well improve access for all legitimate users, but will also allow access to 2-wheeled motorised traffic, and possibly 4-wheeled traffic in places. In the absence of measures to segregate users, or otherwise resolve these matters I do not support the proposals.</p>
<p>Because there is no mention of horse riders who need as many off road riding as possible and the fact that they are proposing to use existing bridleways is a bit of a cheek. All routes should be multi user.</p>
<p>I would support the improvements if they had safeguarding aspects for walkers built-in. A replication of the situation which has been created on the Bridgewater Way would not be acceptable. Here we have a so-called 'shared space' which has become a nightmare for walkers with constant tension between inconsiderate cyclists and other users to the extent that many people are too afraid to use the the Bridgewater Way. I do not include here all cyclists some of us are considerate and respect other users, too many, sadly, do not.</p>
<p>Detailed consideration should be given to the impact of widening existing routes on local vegetation, wildlife and general biodiversity. The impact of the widened paths should be considered on a site-to-site basis, with the option of local narrowing where environmental impact is found to be significant.</p>
<p>Yes BUT please include horse riders in the plans with suitable footing & access. There are over 1000 horses in the local area (more in the whole of Trafford) and there is good usage of the paths/tracks/bridleways. Visiting horse riders also use the tracks . The BHS (British Horse Society) has run Pleasure Rides in the Area over the years, too. The last one being on 14th October 2018 which took on the north & south banks of the Mersey starting from Wythenshawe Park. This is a great opportunity Trafford to be all inclusive (& bring income into the area). The roads are now far too dangerous to ride horses on. Each horse contributes between £3 & £5k p.a , mainly to the local economy.</p>
<p>Please can you make the route a Bridleway open to horseriders in addition to walkers and cyclists.</p>
<p>What about horse riders? There are approx over 1000 horses in the Trafford area that don't seem to be considered?! We do not like riding on the roads, so a network for us should be included.</p>
<p>I am a horse rider and me and my horse use these paths everyday</p>
<p>Disappointed that more lighting isn't included especially on the direct routes such as 08 South East Access. Also disappointed that the direct route from Ashton on Mersey to Urmston Link (Amey Transport committed scheme) is missing as this route requires a hard wearing good quality surface over its entire length (and lighting).</p>

No mention in any of the documentation is given to managing the traffic. The rule MUST be that cyclists MUST give way to pedestrians. Until that issue is managed any path improvements that encourage cyclists to go faster causing more of a danger to pedestrians MUST be avoided.
I am a keen horse rider and welcome access for horse and riders
Yes but please include horse riders in the scheme, we too enjoy riding around the lake
Me and my family are not local residents but regularly visit friends and family in the area. We live in the North of Scotland and are keen cyclists and walkers. We recently visited Sale Water Park and the surrounding area having not visited for many years. At the visitor centre we briefly met two friendly gentlemen promoting the scheme. We were impressed with the current network of paths etc. but fully recognise any improvements that you are proposing will greatly enhance the network providing many health benefits and a better leisure experience. We fully support this proposal and any similar future proposals particularly if they link up providing a more extensive network. Many thanks.
The improvements should also be designed to accommodate horses.
Please make the paths MULTI USER and allow horse access to CONTINUE using them.
Removal of barriers will make these paths even more dangerous for walkers, as we have already seen on the Bridgewater towpath, where cyclists travel at excessive speeds and with contempt for other users.
It is important that Horse riders and Drivers are considered in the improvement scheme
Providing measures are put in place to curb/prevent cyclists from using route as a speedway to the detriment of pedestrians/walkers
footpaths and cycle paths combined are a very bad idea. people of foot cannot use the paths on the canal bank as bikes are travelling very fast. This has in fact reduced the foot traffic on the canal banks.
I would like to see measures in place to slow the speed of cyclists and force them to ride in a considerate manner, (I am a cyclist myself), so the these facilities may be enjoyed by the many and not spoilt by a number of foul mouthed and offensive cyclists who show a completely arrogant disregard for the safety of others.
Taking the Bridgewater canal towpath as an example, too many inconsiderate cyclist regard it as their own personal race track, causing not inconsiderable danger to pedestrians both young and old.
I support the idea of improving access along the Bridgewater Canal and through Sale Water Park for all users but at present everything seems favour cyclists without much thought for other users. They don't use bells to warn of their approach (and that assumes walkers do not have hearing impairment issues) and just shout when you fail to get out of their way. One of my friends was only saved from falling into the canal when she was approached from behind in this manner by her quick thinking husband who caught her arm. My husband who has MS and should take a short brisk walk every day to keep him mobile refuses to use the towpath as he finds the intimidation and assumption he can move out of the way quickly is just too stressful to cope with. Matters are made worse by groups of cyclists all moving together along the tow path. They seem to find it funny that a crowd of people should have to wait on the side of the path while they wend their way along.... More limitations to speed are necessary. walkers really need a separate walkway on account of the rude behaviour exhibited by cyclists you see every day.
For mixed use paths some cycle speed calming measures may be appropriate, for example, approaching blind bends, on gradients, at points where there is a side access, etc., but it is not appropriate to have styles or impediments which should cause the cyclists to dismount. A balance should be struck between the needs of different users.
A great link would be across Turn Moss. This could also link in with a footpath from Turn Moss Car Park to Hawthorne Lane. This would create possibilities for linking up with Longford Park through to the Stretford Cycleway and possibly to the proposed Civic Quarter thus removing a few more cars from the roads..
I am concerned by the removal of barriers and the assumption that walkers and cyclists can share paths without problems. I regularly walk along the Bridgewater canal between Altrincham and Sale, usually in the morning rush hour and the cyclists go at high speeds , quite close to pedestrians, in a potentially quite dangerous and concerning manner and it puts me off doing this walk.. A cyclist has also previously spoken to me in an aggressive manner when walking this route. I consider any removal of barriers to slow cyclists a negative step and also wish to comment that while I am not against improving cycle routes to encourage cyclists they ideally need to be separate routes from walkers as having cyclists going past at high speeds is very unnerving for walkers and it is not enjoyable to walk on a race track. If separate routes are not possible then paths should be clearly delineated into cycle track and walkway and a barrier

between the two sides may be worth considering
In part although it does not appear to include a stretford access into the route
There absolutely MUST be a segregated portion for walkers. I both cycle and walk on the Bridgewater Way. When walking I have been very concerned at the lack of care and consideration shown by cyclists who forget that people strolling (maybe elderly, maybe with poor hearing, maybe walking as a therapy for cares or depression) may change line, move unexpectedly and so forth. The only robust solution is to protect them from being hit by a cyclist. Relaxed walking in landscapes as part of (vitally important) health and wellbeing initiatives must not be sacrificed in pursuit of the (worthy) goal of promoting cycling. Please protect and not exclude vulnerable walkers.
it's really great to not cycle in mud anymore! Love the new look already and the new fencing near to the bridge that crosses over the river near carrington spur. it's become my go to 10 miler off road. LOVE IT
But with changes to the route to enhance the use of Turn Moss
I fully support the proposed cycle route improvements and believe they would significantly improve access for cycling in the Mersey Valley.
Bins and toilets (clean) in the Mersey Valley are needed.
We need as much safe cycling infrastructure across the city and beyond.
Would give access to Stretford, Longford Park and it's cafe to people using the Trans Pennie route
Please remove barriers so that the routes are accessible to ALL users, including wheelchairs, bikes with trailers, cargo bikes, bikes with panniers etc.
Yes, and more of this please.
Need more separate cycle paths . Roads too dangerous
Anything to improve safe cycling opportunities is welcomed.
This needs to happen.
I have received entry stopped cycling and will not allow my son to cycle on his own due to the rise in time and anti-social behaviour. The council's priorities are wrong. You need to tackle the crime first because people (especially women and children) do not feel safe. Just look at the Fallowfield Loop as an example. I used to regularly walk, jog and cycle there. Now it is a complete no-go zone.
I am a horse rider will it be accessible for horses
I do support the route however it should also be made a shared route for horse riders.
Positive to provide more safe cycling routes that can also be used by others not riding a bike. I am a horse rider as well as a cyclist and would like to ensure that path surfaces can be used by riders, or else a wide verge left next to the path, and that cyclists are discouraged from excessive speed, which can scare horses. (cycling alone isn't incompatible with horse riders its speed/ noise associated with speeding)
But only if it doesn't involve the creation of tarmac paths and cycle lanes
Yes but they must include horse access in any proposals
Cycling, walking and horseriding all have healthy lifestyle benefits.they are also vulnerable user groups, so providing /improving routes/bridleways enabling us to avoid using the roads is always preferable.
This could be a significant help for both leisure and commuting.

I support improvement of the existing paths on the N and S banks of the Mersey. In places the existing hardcore is very uneven and adding a better surface would help recreational cycling - but no asphalt please.
Please build in regular maintenance once these improvements have been undertaken. The key thing is to make sure routes connect with other routes to make a network. Please make sure the widest possible path is built to avoid conflicts with users on foot. The improvement work already completed along the NCN 62 under the A56, there needs to be some lighting as it is very dark (understand it is difficult to get power source, but perhaps use of solar or wind power?) dangerous and scary, especially for families with young children.
Inter connectivity between places via non vehicular means should be promoted wherever possible.
These are great, but please also give consideration to pedestrians (and horse-riders where appropriate) as well as cyclists, perhaps with separate lanes or at least marking for the cycle lane and pedestrian sections.
But only subject to horse riders having access as well. Equestrians desperately need safe routes for the same reasons as walkers and cyclists; many roads are no longer safe for them to use due to the speed and volume of traffic on the roads. In the past six years the Society's records show that there have been 2914 reports of road incidents involving horses, 39 Rider deaths, 230 Horse deaths, 840 Horses injured; 85% of these incidents were because vehicles passed too fast or too close to the horse. The proposals do not take into account the 1000 plus horse riders in the area
Improvements to these routes will improve accessibility to green and blue spaces and boost connectivity for multitude of people including people who want to get around by bike, foot, horse and wheelchair. I'm keen to see an fully inclusive approach being taken, ensuring sufficient space is given for people using bikes of all shapes and sizes to include family-friendly options such as cargo bikes, kids in trailers and tagalongs, as well as improved space, access and surfacing for people with mobility difficulties and for people on horse or foot. This means existing barriers need to be removed as part of the improvements to enable wheelchair and trailer access and some paths may need further widening. It only takes one small weak point / obstacle on the route to undermine the rest of the route. For example one of the current routes includes a narrow footbridge, across Chorlton Brook, which is not sufficiently wide for a cargo bike to easily access for example and could create a bottleneck and hazard. To be most effective improvements need to include new cycle crossings and improved signage.
The bollards are a particularly good idea as other types of barriers prevent legitimate access especially by disabled users. I do hope that equestrians are to be included in all of these routes as they are especially vulnerable on roads. I also hope the improvements to surface for cyclists are not to the detriment of the other users.
I am an equestrian and too often these routes give priority to cyclists at the detriment to horses. The surfaces are hard, cyclists travel at speed and it is difficult
They don't take any account of horse riders despite the route involving existing bridleways and tracks currently used by horse riders.
Where are the proposals for horse riding?
But if cycling is permitted then this route should be multi user and must include horse riders too
I think horse riders should also be considered and included in any improvements
I would wholly support this route if horse riders were included in the plans. It seems this vulnerable road user is being forgotten in the proposed plans.
Provided they don't impact equestrian use.
I think the improvements are a great idea but I think part of the route is wrong
But horse riders need to be taken into account. If not correctly done, when cycle routes and bridleways converge it can be very dangerous. Enough space needs to be planned to accommodate both
Excellent wheelchair access over bridge at Jackson's Boat and in Priory Woods.
Please Provide comprehensive improvements to the routes: - wider paths, cut back the vegetation, improve the surfaces for cyclists, remove all barriers to allow free passage of all bikes including tandems, recumbents, tag-alongs plus wheelchairs. Please keep the use of bollards to a minimum. They are dangerous to cyclists.

Please ensure that horse riders can also use this network and that our current bridle paths aren't compromised.
Providing route has separate lanes for walking and cycling
Providing route has separate lanes for walking and cycling
The Bridgewater Way has no segregation for cyclists/walkers and sooner or later this will result in a serious accident. I hope the proposed improvements will address the road for all users to be able to use the route safely.

DRAFT

The scheme, as detailed in the construction document, makes no reference to the proposed (relatively well advanced) improvements to the route known as Cob Kiln Lane from the 'Trans Pennine' (Millennium) bridge across the Mersey through to Meadow Road (just off Stretford Road, Urmston). This should be considered as being equal importance to the whole RIS since without it the Mersey -Carrington Spur section ends in the middle of nowhere and at the north end it is much closer to Urmston town centre. The route from the Millennium Bridge south to Bankly Lane is part of the Trans Pennine Trail and was recently improved but is not in any way shown on the plan as being an important link providing a route from the Sale/Ashton-on-Mersey area both to Urmston and to Sale Water Park. It is recognised that the above may not form part of the U-SWP CRI Scheme and therefore not be included in the funding grant but neither is the 'Improved Section' (un-numbered) linking 05 to 06 and 07 yet this is shown since it is important to the overall scheme. Section 06 is misleading since the coloured band covers not only the north side of the Mersey but the south bank also; the legend box runs to the north bank and the red line defining the CRIS runs along the north bank but if the south bank is excluded the pink shading should be confined to the north bank. Dane Road Access (01) runs between the car park off Dane Road to the point where it passes under the M60. This joins the path alongside the M60 fence approximately half way along its length. Why this section should be considered to be part of the Urmston – Sale Water Park route is irreconcilable; it is the only section south of the Mersey (both Urmston and SWP being north of the Mersey). If a section in this area is to be included, it should not turn south from the fence alongside the M60 but continue in a south-easterly direction alongside the fence to join up with the pedestrian/cycle bridge across the M60 leading directly to area adjacent to the main Water Park Facilities. The Pathway marked on the map leads across open ground and ends up at a car park of little consequence to either Urmston or SWP and has a relatively good surface at present. The section leading to the bridge across the M60 is worthy of serious improvement.

Section 04 – Mersey – Bridgewater Canal appears to run from Hawthorn Lane (Stretford) to the north bank of the Mersey. The section from the bridge across the Mersey down to the bridge under the M60 appears to be unallocated (no colour coding).

The whole scheme is intended to provide an attractive route to Sale Water Park for areas to the west of SWP. An important feeder road to the Park's facilities is that immediately to the north of the M60 from the abovementioned (unallocated) stretch directly into the area around the facilities and the SWP main car park. At the car park end of this pathway there is a gate and other hazards for cyclists discouraging the use of this route for no known reason. The inclusion of the upgrading of this stretch would give excellent facilities both to the north and the south of the motorway between the pathway under the M60 and the bridge over the motorway. What width will the pathway under the M60 be?

Section 08 – South East Access runs from the sluice gates south west to join the road near the heritage centre. This is indicated as being 'proposed footpath width of 3 metres'. It is suggested that this should most certainly, as with all pathways, be shared use path; it is the link between a shared path along the south bank of the Mersey to an established roadway.

It is recognised that the north bank of the Mersey from Broad Ees Dole to the east is under Manchester City Council jurisdiction and it is believed that MCC will be involved in the new bridge at Jacksons Boat. Another very important link to the CRIS is the route available to both walkers and cyclists once having crossed the Mersey by the new bridge. A comment about any proposal being prepared by MCC for facilities on the north bank of the Mersey across the flood plain to Hardy Lane and Chorlton would have been welcome even though they will not form part of the Trafford proposals and funding application. The route along the north bank of the Mersey towards Chorlton Water Park is already established.

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Table 2: Additional Comments given in relation to Question 2 of the online multiple-choice questionnaire

Not sure
Extend the route slightly at Carrington so that it meets Urmston Meadows
Not via Chorlton Brook but instead continuing along the River Mersey north bank. This reduces cyclist-pedestrian conflicts in the Chorlton Ees and Ivy Green area, well used by families and walkers
Need safe cycle routes all across Trafford.
Improving access to Chorlton, coordinating with Manchester Council, down the path that runs from Brookburn Primary along the brook would be great. Currently the surface is poor and path narrow/windy.
As many routes as possible should be improved.
When travelling along the Mersey, direct paths across a meander should be clearly signed, so that riders heading further, such as towards Stockport, can avoid loops along the bank.
The Loretto Road entry to Urmston Meadows is a poor choice for primary funding for several reasons: 1) It's very narrow, very cobbly and is often flooded in parts during the winter months, 2) It's not central to those living in Urmston (being close to the boundary with Stretford, a place from which you can access the route from various alternative locations, not something available to those living in Urmston), 3) It's not a route commonly utilised by cyclists currently due to the above and it's popularity with dog walkers and other pedestrians - there's simply not enough space to pass anybody else, 4) it's mainly used for purely recreational use - in investing in this route you're unlikely to attract any other type of cyclist other than recreational ones. Whilst it would be good to see the Loretto Road path improved the more obvious route to invest in (at least initially) is one involving access via Meadow Road in Urmston. Meadow Road has a number of advantages over the Loretto Road access: 1) It's located more centrally to Urmston, 2) It's already a far more popular route for cyclists than the Loretto Road alternative due to it affording not only a route for recreational cyclists/walkers but also the most direct route for those commuting between Urmston and Sale as they're able to cross the Mersey at that point over the Millenium Footbridge, 3) the path is already wide and is able to cater for cyclists, pedestrians and horse riders (albeit see below), 4) Lots of teenage children from Urmston attend schools in and around Sale (inc.Trafford College) and if the aspiration is to get them to commute by bicycle, this would be the most direct, and certainly the safest route for them to take. That said, whilst the existing access from Meadow Road is very good (and certainly remains the preferred choice for most existing cyclists) there is one very small stretch of it that ruins what should and could be both a brilliant route for both recreational and commuting populations! It's a stretch of no more than 3 - 4 hundred yards leading up to the footbridge on the Urmston side of the River Mersey! It's a mud fest for a large part of the year, but if you were to sort that tiny stretch out you'd have a fantastic route not only over to Sale Water Park, but also for all those commuters (and potential commuters) that'd be able to use it all year round to travel between Urmston & Sale.
As above, this is a leisure route that is already easy to ride, I would rather investment was on roads used for commuting
A link across Turn Moss fields into Hawthorn Lane in Chorlton would open up Turn Moss users to travel there by bike. Currently this is not possible without a wide detour
<ol style="list-style-type: none"> 1. Give the A56 a protected cycleway. 2. Ensure there is secure lighting on this new cycleway - once the clocks turn back it becomes intimidating and scary to cycle around there. 3. Improve crossing of M60 at J6
Please also consider improving and resurfacing the path that runs from the north of Jackson's boat bridge to Hardy Lane and lane to Brookburn Road. This opens up links between Sale and Chorlton. It would be a shame to miss these upgrades as these links are in a poor state Also the path that runs East/west ish, which is along Turn Moss, from the Mersey to Hawthorn Lane. All these links are in a poor state of repair and uncomfortable to ride unless you are on a mountain bike.
As above
Not sure what this means
Yes see above

The section from the pedestrian bridge over the Mersey by the Carrington spur towards meadow road in Urmston. This section is a mud fest even in dry weather and is the worst section on the Mersey. This is where you should be improving and not areas that are already adequate. It also provides a proper off road link to Urmston, unlike your proposed half arsed attempt. I suggest you should get some cyclists involved in your planning of these routes before you put them out for consultation!
Not that I am aware of
N/a
Ones that are suitable for horses
Perhaps a separate cycle lane could be made alongside, separated by a fence.
Don't know the area well enough to comment
As mentioned above, the proposed new path on the North side of the water park leaves the river at Chorlton brook.... but the section at the junction of Barford Bridge floods regularly , and i am keen walker and would like that extended along the top of the river by about 200 meters towards the nature reserve pond , where there is access to Hawthorn Rd. Thank You
all top of river path from Barfoot bridge to first steps down to Hawthorn road, as this floods all winter
Please consider improving the north side of the river Mersey pathway between Barfoot Bridge near the Metro line and the junction with Chorlton Brook, the improvement need not be ' cycle proof' , but fit for pedestrians. This section of the footpath on the river bank is almost permanently under water in winter and in places almost impassable. Thank You
Ensure all any routes include horse riders
Improve the old and unsightly bridge near Chorlton water park instead of the beautifully constructed Victorian Bridge at Jackson's Boat.
Please involved different parties when making your decisions. Discuss it with the different groups. Look at everyone's needs
Have no idea as not a resident to area, only visit to use bridleways.
I have spent many years publicising what little safe and/or off road riding. I used to ride frequently in the area and I know how many riders will be affected. In 1966 cyclists were allowed to use bridleways. Is it not time designers of 'cyclerooutes' acknowledged that a track can safely be multiuser? I developed a 33 mile route in Cheshire that is happily used by walkers riders and cyclists; we have a tracker that proves this.
Not sure
Please make it horse friendly. Riding on the roads is so dangerous these days
Accommodating all existing route users should be factored in, not just those who are following a current trend.
Links for multi user tracks as above
yes make it a bridleway
None I know of
Provide a separate cycling route for cyclists only. The shared path doesn't work & is dangerous.
I would actually like to see the fast cyclists separated from the towpath completely. With a barrier between the cyclists and other path users. Possibly a fast cycle lane following the Chester road/ Washway road. And one following the M60.
From Flixton - Isherwood Road using Restricted byways 21 & 23 up to the Carrington Spur across Banky Lane/Millennium Bridge.
The number of safe, vehicle free routes for vulnerable road users is limited both in the area of this improvement scheme and nationally. Walking, cycling and horseriding all provide exercise and a means by which individuals can access the outdoors key to physical and mental health and all encouraged by healthcare professionals and the NHS through their One You scheme.
Safe options please and an Equestrian representative consulted.

Additional route: Disappointed that the direct route from Ashton on Mersey to Urmston Link (Amey Transport committed scheme) is missing as this route requires a hard wearing good quality surface over its entire length (and lighting).
Equestrian access at Jackson's Boat and over the new bridge.
Don't have enough local knowledge to comment.
There are few safe off road areas for horses nowadays especially in urban areas. Cyclists/walkers and equestrians can equally benefit from thus investment. Please do not exclude consideration of bridle paths or shared routes in this scheme.
From Turn Moss playing fields to Jackson's Boat.
Hawthorn Lane would be an ideal place to route a cycleway to Turn Moss.
A route from Turn Moss Car Park to Hawthorn lane would bring additional benefits. This would open up the route to many cyclists who could access through Longford Park and Turn Moss Road. This would provide a traffic free route from the Northern end of Trafford to the new cycle-way.
At Millenium Bridge there should be improvement work to the Ashton Upon Mersey link. I would argue that there's an even greater case for the shorter Urmston to Ashton Upon Mersey route than onto Sale Water Park; although ideally I'd want both. At the moment the condition of the track particularly closest to the river is almost impassable due ironically to the construction work on the Millenium Bridge.
It would be good to be able to cycle from Longford Park across Turn Moss, then on to Hawthorn Lane. At the moment even with a cycle lane it's not particularly safe cycling along Edge Lane.
Maybe an access point through turn moss car park using the current right of way or maybe from Bridgewater canal onto hawthorn lane stretford. I would hope that the cyclists part of path will be sectioned off as some do not take into consideration walkers that are hard of hearing and therefore do no know they are coming until they have either hit them or a near miss I would appreciate confirmation of this thank you
There simply MUST be a link between Edge Lane in Stretford and Hawthorn Lane, in Chorlton, which should run across Turn Moss.
A link between the car park at Turn Moss and Hawthorn Lane would benefit cyclists and walkers alike
A link to the car park at Turn Moss and improving Hawthorn Lane rather than using the path along the brook, which gets busy with dog walkers.
over the meadows in urmston, there's a route there which is full of brambles crossing from the TPT coming out near urmston cemetery and it's a shame not to be able to use it.
It would be fantastic to link Chorlton to Stretford via a link path through Turn Moss. It would provide a scenic and traffic free route which I'm sure would entice more people to cycle / walk / run through it
Consider a route across Turn Mos from the car park towards Hawthorn Lane. This lane would require minimal upgrade and provide better links into the other networks on the Manchester Trafford area especially north toward the city centre.
It would be good if access to Turn Moss could be included in the proposals, given that the improvements are so close by.
a link between Turn Moss car park and Hawthorn Road
I say no but I live in the city centre but commute daily by bike to the mall, when I go out riding I usually go out passed urmston so any improvement is welcome. As pleasant as it is on the canal it's full of people with strollers/dogs etc so not convenient for a ride out, unless the paths are widened.
From the bottom of Turn Moss Road (Turn Moss car park) to Hawthorn Lane. This will open up access to the Mersey Valley from places like Longford Park. Giving lewisite cyclists more possibilities away from streets and main roads.
Linking sale water park (or rifle road) with a lit route through to Chorlton so it is safe to use all year round.
We would like to see a cycle route from Turnmoss carpark through to Hawthorn Lane.
Please add Turn Moss
I suggest a Turn Moss linking pathway Turn Moss from car park to the River Mersey.

Link from Turn Moss car park to the footpath (Hawthorn Lane)
A link to Turn Moss would be fantastic.
To create a link across to Longford Park and Stretford via Turn Moss. Creates a fuller network of cycle paths with a variety of access points. Also reduce cycle traffic in routes already well used by walkers.
There should be a path connecting turn moss to the river Mersey
A route through Turn Moss, via Hawthorn Lane, should be used instead of sticking to the river. This creates green link possibilities with Longford Park, the Civic Quarter in Old Trafford, and the proposed Stretford Cycleway. It would also make cycling more accessible to people living in Chorlton.
I'd like to see a path from the Turn Moss car park to the River Mersey. It would be great to have a path on Turn Moss. Fields would be a lot more accessible for older visitors, families with buggies, spectators, easier access to pitches closer to to Chorlton Brook.
Turn Moss to River Mersey would be ideal
Link from Turn Moss Carpark to Hawthorne Lane
Access through Turn Moss will open up the routes to many more users who will have much safer travel from the surrounding areas.
I have ridden my horses around a number of these tracks and bridleways for many years and while the development and improvement of access is great there is no provision or mention of retaining access for the many horse riders who use these tracks. Can provision be made to include horse riders on this route to allow us to continue to enjoy our fantastic outdoor spaces across Trafford.
I support a route across Turn Moss as suggested by a lot of cyclists.
Edge lane as cycle routes will infringe upon pedestrian walkways
We would like to see more bridleways there are many fantastic dog walks but not many horse areas. We are near Dunham park and only have transpennine way. A circle route would be fab
Turn Moss link from car park to River Mersey
A Im additional route through turn moss, as supported by the Friends of Turn Moss
1) The path on N side of Chorlton Brook floods in winter and is then completely impassable. This make it unsuitable as a year-round cycle path. I support the alternative route along the N bank of the Mersey. 2) Much attention is given to access to the cycle route from the Bridgewater Canal. I support an additional access route from Turn Moss car park to Hawthorn Land (and hence to the N bank of Mersey path).
Route 02 and 03 from Jacksons Boat misses the chance to rectify a major flaw in the NCN 62, namely the very narrow footbridge which crosses Chorlton Brook. This footbridge needs a major upgrade to allow bicycles and walkers/prams/wheelchairs to comfortably cross together. At this point the route should head north, through Ivy Green, to allow a connection with the (already bound tarmac) wide route of Hawthorn Lane at an earlier point than the proposed route. Keep the path along Chorlton Brook for foot use only, it is not an easy job to widen this path to 3m due to the brook on one side and steep bank on the Northern Slope. Once joining Hawthorn Ln via Ivy Green, it would be brilliant to construct a short bound path across Turn Moss fields, to connect with the Car Park and exit via Turn Moss Road. This would open up a green route for southern population of Trafford (Brooklands, Northern Moor, Sale) with the northern areas of Old Trafford, Firwood, Gorse Hill, Stretford. Utilising a safe connection with the cycle/shared signed network already within Longford Park across the road. #Beelines is proposing strengthening this connection with a Toucan crossing from Turn Moss Road to Longford Park entrance on Edge Lane. This is too good an opportunity to miss! 06. Carrington Spur. It's a short section of work which would make a big difference and connect up Ashton on Mersey to the network would be at Carrington Spur where it crosses over the R Mersey. If you take the cycle bridge onto the S side of R Mersey and then go immediately SE under the spur, there is a path and it takes you right to the centre of the village. It is prone to mud, but if fixed (only a short section) this then travels South along the edge of the golf course coming out at Ashton Hall Equestrian Centre at Church Lane and opens up into Ashton On Mersey village.
There are two important ones that would represent an excellent use of the available funding and would fit very well with the main Transpennine Trail plans:
1) A well-surface cycle and pedestrian path from Hawthorn Lane (on the Manchester/Trafford border) northwards through Turn Moss. There is already a right of way for pedestrians but this runs across

grass, heavily prone to flooding and muddiness, and then along the route of Turn Moss Road, which is very narrow, poorly surfaced and is currently also used by cars accessing the football grounds. Ideally, there should be a clear cycle lane and footpath along the length of Turn Moss Road to the car park and then:

- a) A better surfaced footpath from the car park to Hawthorn either due south or south west, or along the existing RoW, and
- b) A separate asphalted cycle track from the car park to the south east, towards Jackson's bridge.

The advantage of both of the above (or failing that, a surfaced combined cycle track/footpath from the car park to Hawthorn Lane) is that it bridges a 'missing link' between cycle routes (shown on the Sustrans national map) through Longford Park to the north and the Transpennine Trail in the Mersey Valley. Bridging the missing link will open up traffic-free through-routes for both walkers and cyclists from the populated areas in north Stretford and Chorlton (either side of Longford Park) and even from the new Civic Quarter, and the Bridgewater Canal beyond that. This also forms part of the projected GM Ringway long-distance path, for which the involvement of the GMCA and TFGM is being sought (see route on <http://gmringway.org/stages/1-manchester-to-sale-water-park/>).

2) If funding permits, there is an urgent need to improve a section of the Transpennine Trail to the south west, from Seamon's Moss Bridge (Oldfield Brown, Altrincham) northwards to the west of Broadheath and Sale. In particular, the sections along Seamon's Road, Dairyhouse Lane and Sinderland Lane are currently all along a public road, meaning that cyclists and walkers have the share the route with speeding traffic, including around some blind corners. If this section cannot be funded in this batch, that section should have a separate cycle track and footpath (or pavement) added as part of the Beelines funding.

Connection from Longford park through Turn Moss to Hawthorn Lane

An additional cycle route should be created through Turn Moss Playing fields. Not to do so would be a missed opportunity as just a very short stretch of additional cycle way (approximately 150 meters) linking Turn Moss car park/Turn Moss Road and Hawthorn Lane, is needed in order to connect the improved TPT routes with existing cycle routes through Longford Park for example and in the surrounding area. This will in turn then connect up with the key cycling routes in and out of the city, to include the proposed new Chorlton Cycleway and with the Stretford Cycleway and new proposed traffic free "Civic Quarter". This will make the most of the existing infrastructure, including Hawthorn Lane as an accessible route with a good surface, and will help create a more integrated, high quality routes that is safe, direct, connected, comfortable and attractive, linking with wider cycle networks. This would open up green routes for residents across Trafford, Manchester and local areas.

any links to nearby housing or roads should be considered as people walking or riding prefer to take the shortest route possible if commuting.

I don't know

I don't know of any

Cycle path across Turn Moss

A path connecting Turn Moss car park with Hawthorn Lane

1. The route along the north bank of Chorlton Brook (between the stone bridge and the metal bridge) is much used by walkers - the proposal will encourage many cyclists onto this path, even if it is wider then i think this is dangerous. This route will lead cyclists to the very narrow metal bridge by the Brookburn Lane car park - this is a bottleneck already with dogs, kids and buggies - it will become a dangerous bottleneck when cyclists are added to the mix. How about taking the new improved cycle route along the north bank of the Mersey instead? There is already a double path there so cyclists and walkers can be kept apart and everyone will be safe. It is so important that there is a balance between walkers and cyclists - the proposed route along Chorlton Brook will create a fast cycle path that endangers walkers and ultimately may reduce the total number of people enjoying the area. Please consider the alternative River Mersey route. 2. There are opportunities to connect the Mersey Valley with other parts of Trafford. - in Ashton on Mersey a connection from Church Lane to Millenium Bridge (not via the rugby club , over the bridge and under the spur) would be brilliant. 3. and on Turn Moss a cycle path from the car park to the Mersey (by the gas sub station on Hawthorn Lane) would connect Longford, Gorse Hill and the Civic Quarter with the Trans Pennine Trail and the Mersey Valley. Please consider the rerouting

of the Chorlton Brook proposal and the two 'feeder' paths at Ashton and Turn Moss.

A link path from the car park at Turn Moss to the River Mersey, linking Longford Park and Gorse Hill with the Mersey Valley route and Trans Pennine Trail.

Towards Chorlton

There are two routes: 1) a route from the NCN62 to Longford Park – achieved by extending TurnMoss Road southwards to join the Ivy Green area paths. 2) improvement to the track which runs from the NCN62 at the Carrington Spur/River Mersey bridge to Meadow Road in Urmston

Improved access from Longford Park to Turn Moss and Sale/Chorlton Water Park would improve access to the routes for Stretford, Firewood and Chorlton residents.

A cycle route connecting the cycle routes through Trafford and Longford Park with the Trans Pennine Trail, that avoids roads (though Turn Moss).

Would be good to put a route across Turncroft Moss

Please See attached Sheet

A connecting cycle route to link Longford Park & Hawthorn Lane is very important.

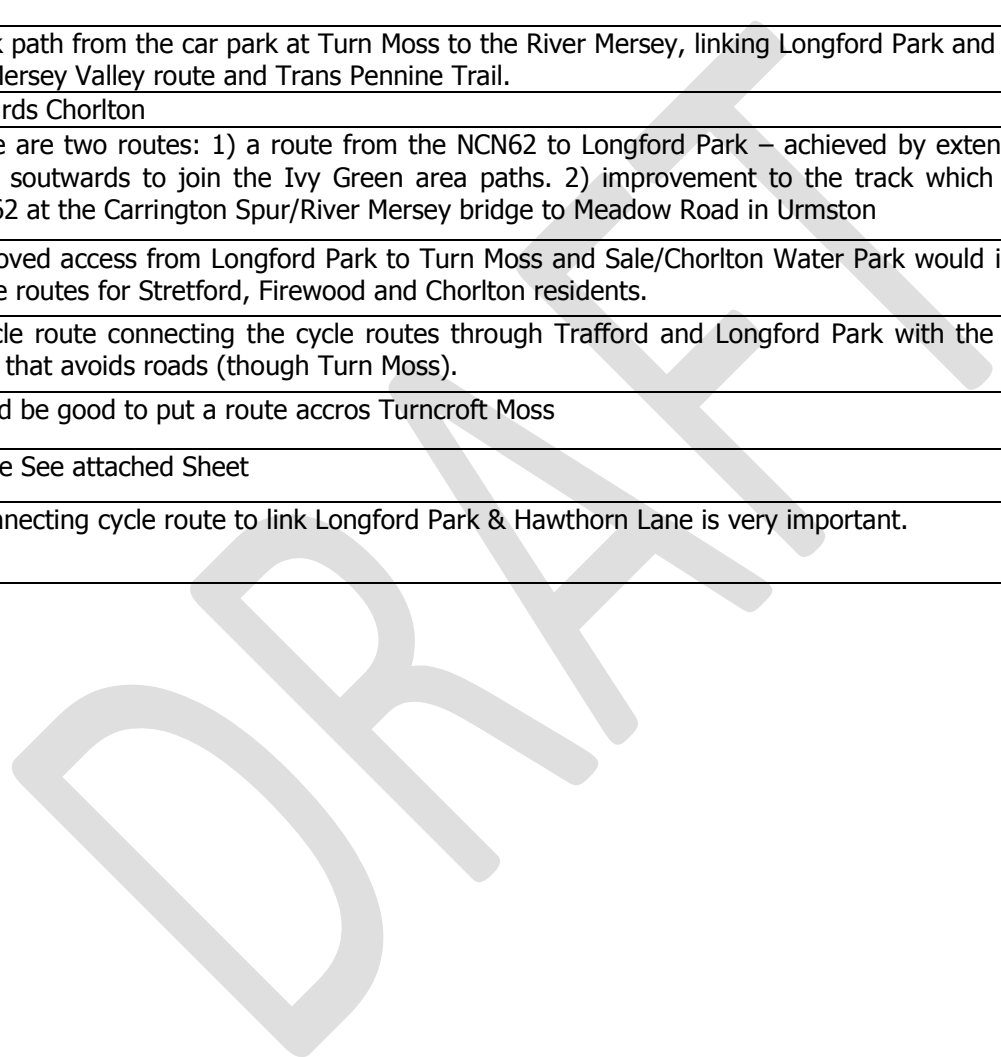


Table 3: Additional Comments given in relation to Question 4 of the online multiple-choice questionnaire

This is an area with a natural feel and no street lighting. Asphalt is too much like a built up area; aggregate stone is less suited to a cycle route
Bound surfaces are a must for all-year round usage.
Asphalt please, otherwise the route is limited to mountain bikes. Please keep in mind accessibility to wheelchairs and pushchairs.
Anything other than proper asphalt like on the Fallowfield Loop is just not pleasant to ride a bike on at the best of times (dry, sunny) and is downright debilitating to all but rugged mountain bikes when wet. Please ensure you only use the highest quality asphalt here. There exists beige/brown asphalt that might offset fears of "urbanisation" that would crop up from a proposal to use regular black/grey asphalt.
Best to have bound to avoid muddiness and puddles. Flatness and possible cambering are important considerations.
Ideally should be possible to ride using road bike/narrow tyres and not require an MTB type of tyre
The trail should maintain a natural look in keeping with surrounding s
Asphalt allows the most types of bicycles (and push scooters) the best surface and limits punctures and other bike damage from road debris
This gives best surface for different cycles. An unbound aggregate stone has been used for cycle path improvements in Salford and I have found it is not conducive to cycling at all.
Prefer a smooth firm surface without using any small gravel pieces. It should be comfortable for wheelchair, pram and mobility scooter use
Some surfaces are not safe for horses with shoes so the less smooth the better
No preferred option but its essential that the upgrade will last.
The present surfaces are fine - just a bit muddy at times.
As Q1. In general most of these routes are perfectly adequate currently and there is no need to upgrade them significantly. Use if tarmac would lead to the conflicts currently happening on the Bridgewater canal since its upgrade of time trialling road cyclists vs pedestrians and animals. The off road surface is also much more beneficial to the joints etc of people who use it even if not so favourable to a bike!
I ride horses so any surface that is not slippy or stoney.
Ensure it is safe and hardwearing
I would use all of the proposed routes
Semi-bound gives the best grip in all weather conditions (tarmac tends to get icy)
Suitable for horses - not loose stones or asphalt. Need softer surfaces tried and tested on other multi use tracks
None of the above, rubber/polymer compound is more environmentally suitable, cheaper and longer lasting - as used by Lancashire County Council
Needs to be suitable for horses
Although smoother surfaces are good for cycles. They are encouraging cyclist to travel at faster speeds that are not safe for walkers, dogs or horses & riders. Also, these hard surfaces are in fact causing foot problems for some horses.
The best would be one that would suit horse riders as well as cyclists. An anti slip surface is best
Asphalt is not aesthetically pleasing
Suitable for horses Non slip
Tarmac is unsuitable for horses in some weather conditions. Please allow us to continue to ride safely.
A flat surface rather than gravel is better due to my horse being barefoot.

May I suggest another compound which has been used by Lancashire County Council where the tracks are Bridleways. The product is nu-phalt and is superior to Asphalt the contact is Richie Collins. I have no financial links with this company or product.
As some of the proposed route will use existing bridleways none of these surfaces would be suitable for these sections. I suggest you contact your local Endurance GB NW group or British Horse Society for input. These bridleways are regularly used by both these groups.
Must be appropriate for equestrian use in all weather
Horse suitable preferred as this would make a nice loop for us.
Surface on Mersey south side between Jacksons Boat and Bridgewater canal is very inconsistent. Hard packed gravel/dust sections are great, but rocky "builders rubble" sections are awful. Asphalt would be an improvement on current surface, but loses some of the charm of a riverside path.
Maybe ,,,,, VERY BIG signs for cyclist to follow the correct code on a shared footpath with pedestrians... as when this path is complete alot more cyclist will use the path.... naturally.... but as i find on my daily walk in this area (river and Canal) few cyclist have bells and or use them. Thank You
just the best and most long lasting surface , especially in the most used areas... etc .. near Jackson Boat Bridge etc.
No tarmac, terrible surface for many users and make path unusable as hot and cold time of year
Whatever can cater for horses too
Please ensure the surface chosen is suitable for the horses that use this route regularly.
Sorry, not really sure which bits are which so might have got preferences wrong. When I've visited it has been on organised fun rides with others planning the route.
Consider horse riders
Something that won't freeze easily and can also accommodate horse riders
A non slip surface for hooves.
Bark - please consider the horses feet
Surfaces should take into account horse riders and not be slippery. No asphalt.
All of the surface options wouldn't be suitable for horses.
Surface suitable for horses
The surface should be suitable for cyclists, walkers and horse riders in all weather's.
Must not be slippery and therefore suitable and safe for horses
What about a surface appropriate for horses.
It makes it more stable for horses
Need to consider horse riders & eants safe for horses. Asphlat is slippy for horses.
See comments above about horse riders being included in the proposal. Surfaces should be suitable for horses (shod and unshod)
This track is used by horse riders. I strongly object to a hard surface being used. There are few enough off road tracks with a good horse riding surface as it is.
Something Suitable for horses
Needs to be safe for horse riders to use
Something that isnt slippy with some give in it to stop concussion of horses joints. And lesson the impact of a fall.
Any. Just make it horse friendly route
Surface along kickety Brook is suitable for horses and, cyclists and walkers. A similar surface would be good,.
All surfaces must be suitable for horses. Asphalt is dangerous for horses and causes slipping and falls.

Urbanisation is not a great way forwards in semi-urban environments. People use current routes because they enable them to have some peace and distance from built-up areas. More asphalt doesn't equal progress.
Please ensure the surface and any barriers used are suitable for horses as well as cyclists. Cyclists are not the only people that use these paths and consultation with other users must take place to ensure that everyone has equal access
asphalt is not ideal for horse riders and encourages cyclists to go at speed. Where horse access is permitted perhaps a grassy margin could be left so they can choose the preferred surface. Many horses do not have shoes for health reasons and prefer a softer surface.
tarmac is too slippery for horses
Yes for horse riders as well as cyclists, it can and should be communal. So many horse riding tracks are being absorbed by tarmac for cyclists, a bit biased I think.
Recycled rubber works well and is economical .NuPhalt
Make surface suitable for horses
A safe suitable surface can be achieved for all users. Tarmac surfaces encourage cyclists to speed and put other users at risk
Leave the path as a natural surface for walkers. Have a separate surface for cyclists.
large signs advising cyclists to have some respect for other path users
A carefully designed path needs to be used to allow for all users - pedestrians, cyclists and equestrians. There needs to be a surface suitable for equestrians including areas for cantering where possible.
Surface should be suitable for all users, including horses.
A surface which would suit all users of PRW, including horses
Consult with British Horse Society about the best surface, some of the work done on the lower Mersey bank from Chorlton on the Sale side is very stony and not great for horses. Asphalt is not good for horses, have a more natural surface
I chose this option as the preferred for equestrian users however I am aware that not all of the route will be accessible to Horses. From a cycling point of view my leisure bike will cope with any surface.
Needs not to be stony to accommodate horses
As long as it's had been approved by BHS for horses to walk safely over
Surface treatment should be appropriate to a shared route, and in particular should discourage high speed cycling, which is a source of complaints on the Bridgewater Canal towpath (in spite of unsuccessful campaigns to improve matters).
Surface treatment should be suitable for horse riders as well as cyclists, in particular on those routes designated as public bridleways, as well as more generally.
NOT TARMAC as this presents an ice like surface to horse riders leading to costly falls both to horse and rider. Unsure re the other 2 options (opted for unbound). However, the recently improved surface at the Millennium Bridge (good surface and gradient) and the section of the TPT running parallel to the Spur Road (A6144) is excellent. For Info - There is a rubber type compound called Nuphalt which Lancashire County Council have used to good effect . Cheaper than Tarmac I believe. which may be worth consideration.
Please use a surface to enable access for disabled users (wheelchairs and motorised wheelchairs)
A surface suitable for horses, not slippery asphalt.
Anything suitable for horses
Unbound lasts less long and needs more maintenance. In Winter unbound gets muddy . In summer unbound is a dusty surface which is irritating especially for runners and walkers

However not the most expensive option if you have to sacrifice some parts of scheme to maintain budget. That said the surface treatment needs be durable, long lasting and easily maintainable.
Definitely NOT tarmac. Rubber compounds such as Nuphalt are cheaper than tarmac and are preferable for all users. The cheap grade Tarmac generally used on paths such as these are extremely slippery for shod horses, even those with road nails, and horses regularly fall over, causing serious injuries to both rider and horse.
Asphalt will make it slippery for horses. This could cause falls and injuries to both horses and riders.
Create an all year round all weather surface
a natural compacted earth surface is good enough for walking - and cheaper!
people with limited mobility need stable surfaces in order to make use of such paths. A bound surface would also make access easier for wheelchair users.
Road bikes work better on Asphalt
I am not sure about this.
I would use a route in from stretford re previous question
Asphalt is great but a surface that generates a bit of noise gives extra warning of approach from behind
Needs to allow cycle. horse and pedestrian access.
A bound asphalt surface would allow more types of bicycles / prams / wheelchairs to access it
Edging of the path is what?
I think it's essential that a bound surface is used, so the a good quality surface is maintained for a long time with little maintenance.
You wouldn't build a new road out of gravel, so please don't make new cycle infrastructures out of substandard materials. If necessary build less but of higher quality.
It mneeds not to be muddy.l
The Bridgewater canal path surface is far too loose, you feel like you're going to slide into the canal which will s unpleasant and has (along with the bad attitude of dog walkers) deterred me from using it, even though living on Whitworth st and commuting to Stretford six days a week this SHOULD be my ideal route.
Would allow wider group of cyclist, however, any option better than none
Smooth as possible. Eco friendly as possible. Wide as possible.
No preference.
Asphalt is a big no for me! It encourages bicycles to go too fast. I would suggest option 2 or 3.
No preference
I think it would be useful to link acyclr track across Turn Moss from Longford Park to Hawthorn Lane.
as stated above there are a lot of leisure horse riders in Trafford using this area whose views also need to be considered. Tarmac is problematic for horses although cyclists especially those travelling at speed love it.
The use of them carpark mats which allow the grass to grow through them to hid them
Suitable for horses
An area of the TPT at Kickety Brook was resurfaced (july/aug 2015) after initial path improvements were made. Due to incidents of horses slipping a short section of sloped Tarmac had to be replaced, this alternative surface has proven to be much more suitable.
Turn Moss link from car park to River Mersey will provide a good route to Longford park.
It needs to be weather proof and rideable on a road bike.
Don't want asphalt, as we want to preserve the semi-natural aspect of the area.
I regularly use this route in any event and would prefer to keep the usage in keeping with its 'natural' format. Tarmac in my view would be inappropriate and difficult to maintain in the long term.

I understand that this is best for cyclists. It would also be good to have a softer surface where possible for walkers.
Preference is for a bound surface and for a recycled option, e.g. some bound recycled tyre based surfaces recommended, must be smooth and durable. Paths need to be 3 meters. Important it is laid properly so rain runs off and the edges are properly finished.
Asphalt freezes in winter so should be avoided at all costs or all of the routes will be unusable, unbound stone is unpleasant for all users walking riding bikes or horses, so the only logical option is 3, 20mm down to dust is best.
I prefer an asphalt surface as it offers better drainage
Asphalt is slippy and often unsafe for horses depending on the particular type used. Unsuitable for existing bridleways
Must be safe and suitable for horses not sealed not tarmac
Please consider horse riders - I don't like a very rough or stones surface
For the proposed route to be safe for all users the recycled aggregate/dust compacted would be the preferred surface
Not sure - whatever is best for horses as well
Suitable for horse riders without it becoming too muddy in wet conditions
THE ROUTE NEEDS TO BE SUITABLE FOR HORSE RIDERS AS THIS IS A REALLY POPULAR RIDING ROUTE. WE CAN'T RIDE ON ASPHALT ON SHOD HORSES AS IT IS TOO SLIPPERY & THUS DANGEROUS
Wheelchair Friendly is best. Also consider what is easiest to maintain.
Option 1 sounds like the best options for wheels, as well as pedestrians
Please ensure that horse riders needs are taken into consideration
I do not know much about these surfaces but believe that asphalt is best for bikes
The best surface for cycling and that will resist erosion too.
Only asphalt will be capable of being satisfactory marked with a line of demarcation to indicate pedestrian and cyclists lines on shared use paths. Shared use paths that do not indicate the line that should be taken by different user groups are a 'recipe for disaster' as has been found on the Bridgewater Way.

Table 4: Additional Comments given in relation to Question 5 of the online multiple-choice questionnaire

The current bridge needs maintenance to the deck now, as construction of the replacement will not be for another 18-24 months
Current bridge is very narrow, barely crossable on bike even when no-one else is around.
The bridge should be fully accessible to people in wheelchairs.
Yes. The current bridge, historic and attractive as it is, is not suitable for walking, let alone walking and cycling. The proposed 6-9 month period with no crossing should the bridge be repaired is unacceptable too. Is it possible to relocate the existing bridge to another location that currently doesn't have a crossing rather than scrapping it completely? It would be a shame to lose the history.
The current structure is viewed affectionately so if it can be retained alongside a new build or re-used somewhere nearby that might assist.
Avoid need to dismount bicycles or horses. Create straight approaches not twisty.
Current bridge is sub-optimal even for cycling
I'm not sure that I understand enough about the reasons for the existing structure being removed other than it's clearly unsuitable for wheelchair users. That said, the land either side of it isn't that attractive to disabled users in any event so I'm unsure what additional utility will be derived from merely replacing the bridge. I don't have strong opinions on this part of the project.
The current bridge is only suitable for one way travel and difficult for both pedestrians and cyclists to use at the same time currently. Its very well used and is a much used commute route of mine
Really don't like the colour scheme proposed (light and dark green). I think it is a mistake to use these colours under the assumption that they will 'blend' with the surrounding greenery - they won't and will also look very dated. It would be preferable to use a simple materials palette of timber and neutral grey/stainless steel/galvanised steel to maintain a clean, contemporary appearance that doesn't jar with the surroundings.
I ride over the present bridge on a regular basis and my horses are fine with it Some may spook at the surface however and the access near Jackson's boat side isn't too good
Useable at present, other areas that the money could be spent as a priority. See above.
The present bridge is fine.
The bridge steps and ramp are part of the area's history. A new bridge will be out of character and a total waste of money.
There really is no need to change this bridge. Strikes me as spending money for the sake of it and to give you a photo opportunity demonstrating 'improvement'. In these times of tightened spending I'm sure there's better ways of spending money.....
I prefer the Truss style bridge.
This is required and will be an attractive addition and draw more people to the area
The current bridge is falling apart and is not accessible to all
Providing that horse riders are included
Again as long as it's suitable for horses and walkers. Also in regard to dogs and small children I think a bridge constructed like either the white millennium bridge or the green bridge with mesh along the bottom rather than holes will be safer.
Equestrians should be included in this proposal
Horse accessible
Please can it be horse safe
Please ensure it is suitable for horses to cross.
However think the current bridge should be kept for pedestrians
Yes but without destroying the old one
Better cycle access would be great.
Only if the current historic bridge is preserved for pedestrian use. Preferably, the new bridge should be sited in such a way to allow upstream and downstream views of the old bridge.

Please construct new bridge before removing old bridge if at all possible as having no bridge will severely impact on people getting from Chorlton to Sale... the Present Bridge is used MASSIVELY.
Yes brilliant idea Will encourage more users as enables them to change routes. Also great for local businesses.
All users is good
Consider horse riders
Must also be suitable for horse riders
All users including horses.
Needs to accommodate horse riders too
It should be suitable for horse riders
The money which it would cost to create a new structure could be better used to repair, paint and if necessary build up the sides of the Victorian Bridge.
Please consider horse riders
As long as this is an access for all
So long as riders are not excluded. Whitfield nature reserve has excellent bridges as a model.
As long as out can be used by horses
All users including horse riders
Please make it suitable for horses
Horse friendly
Can horses use it?
Please make it horse friendly
Needs to be accessible for horses and riders as well which it isn't now.
It would be a huge improvement to allow horses to cross the River Mersey at the point of Jackson's boat
See above.
will it be safe for horses
Unfamiliar with this
Make sure it is suitable for horses
Not if this gives priority for inconsiderate cyclists.
No I like the old bridge, it does its job very well and has history in the area.
Provided that it has a non slip surface and handrails of a sufficient height
Make it horse friendly.
Although I note that there are no immediate plans to change the route designation across to bridge to a bridleway, the design should be such as to be suitable for horse riders in order to future-proof the bridge in case there is a desire to change the route designation in the future.
Only if it is made for horse riders
Yes, again provided adequate safeguarding for walkers is in-built to the scheme.
The existing bridge is a historic landscape feature of significant cultural value. It should be retained and refurbished despite its limitations as a cycle route. There is no objection to the ramp access being improved, but there is no reason why cyclists should not dismount and walk across the bridge. Consideration should also be given to a third option of retaining the existing bridge and building a new, wider structure in another location -providing a new crossing point mid-way between existing bridges. Alternatively, consideration could be given retaining the existing, historic bridge and attaching a new pedestrian/cycle bridge to the Metrolink bridge.
I support the principle for ALL users (as indicated in the question) to include horse riders. If you are going to spend x amount on a new structure then why not make it all-inclusive.

Horse ride along the Mersey basin, access across at Jacksons Boat to include horses as well as cyclists would be much better than having to go all the way down to Chorlton Water Park.
Yes. For horses.
But not at the expense of quality routes and surfaces elsewhere. The cheapest option should be chosen if any of the routes is at risk (including Ashton on Mersey to Urmston Link an Amey Transport committed scheme see answers to Q1 and Q2).
The impact on the popular Jackson's Boat pub isn't given. If an impact statement was provided that showed no impact then would reconsider.
Please make it horse friendly
Please make it horse friendly
Ensure gateways and access points are suitable for horses, and parking available for horseboxes etc.
The existing bridge is perfectly good.
Dont see the need
old bridge is historic and should not be removed
The old bridge should, if possible, be retained as a footbridge only. It's part of the history of Jackson's Boat.
I wont be using it but any improvement I will be happy with
Improved access to the bridge would be welcome.
Could the old bridge not be renovated? It is such an iconic bridge
I am not sure what is wrong with the existing bridge with improved ramps for access
I would keep current bridge for pedestrians use only unless the new bridge has a separate lane in it for cyclists fir tbe safety if pedestrians
You're asking a lot of questions some (like me) may not be able or qualified to answer with certainty.
Needs to be wide
Old bridge past its sell by date too narrow and access poor.
There should be a new cycle bridge but retain the old one two bridges one at a different point. this would provide a diversity of routes.
While it would be good to keep the existing bridge, it is no longer fit for purpose. The benefits that a new accessible bridge brings, far outweigh the loss of the existing bridge.
Improve access for all - without barriers.
the old bridge is accessible enough and beautiful and original and an important local landmark - do not take it down! Leave it alone
Unsure of details, would not like to create new access points to detriment of existing historical bridge points.
Not if it means closure of the current bridge though
Depends on the bridge design and cost.
This is unnecessary and a waste of money. Cyclists should be made to dismount for the two minutes that it takes to cross the bridge. A new widened bridge will only encourage more speed in an area which is already hazardous due to cars and children/dogs coming down from the riverbank or bridge. It will be dangerous to have cyclists riding across the bridge. I own a narrowboat and the Bridgewater Canal towpath is currently a nightmare due to inconsiderate cyclists doing speed trials into town. A lot refuse to stop to allow for mooring of the boat, creating a hazard and shout obscenities as if the towpath is purely their domain. The same thing is likely to happen if this area is promoted as an alternative commute. The area should be preserved as primarily for leisure.
The bridge should be of suitable width and height for pedestrians, cyclists and horse riders.
it would be good to be able to cross on a horse, the current bridge is barely wide enough/ not practicable and currently the base isn't secure enough.
No they can walk the bikes across the bridge so why build another bridge when the monies could be used on other areas of the route
Not metal surface on bridge pls
Suitable for horse access

The new bridge design would be very suitable and really beneficial to myself and other local horse riders who regularly use the mersey valley. It would provide a different route option which is greatly needed in this area, helping to keep us off the roads.
I have never had issues with the crossing although it is steep & narrow for some users
This area has a historic aspect, with the old Jacksons Boat pub next to the existing bridge. The existing one is has more character and is worth preserving. It is only the wooden planks that need work, which is surely cheaper than a new bridge.
The ramps and railings on both sides of the river should be improved aesthetically (as compared with the plans and illustrations provided). The new bridge design looks fairly attractive, but the harsh unpainted metal bars and gates, combined with a lack of landscaping would create an unpleasant urban appearance in what is an attractive green, rural-looking setting.
Subject to it being available to horse riders as well.
Given the heritage value and appeal of the existing bridge our preference is to see the existing bridge retained, repaired and maintained possibly as a footbridge with a new bridge providing a separate cycle way. If it is genuinely not feasible to retain the old bridge then our preference is to replace with the bridge closest in design (Warren Truss Bridge). The existing bridge should be retained in working order until the new bridge is fully operational.
Yes a new bridge is a good idea, not sure why anyone would want to use steps when there is a ramp, so maybe save some money and just have ramps. I especially would like to see this as an equestrian standard bridge as presently equestrians use both sides of the river at this point, it would be really great to be able to cross here, especially when I am leading my 7 year old granddaughter on her little pony and I am in need of a rest and a drink or even just a shorter route back to base. At 4 metres wide this shouldn't present a problem, have a look at the Chadkirk bridge in Stockport.
All users - including horses
Please keep the historic bridge
Please make sure horse riders can use it
At the moment the Manchester side is accessible to wheelchair users but the Trafford not. i would support proposals that make both sides of the bridge accessible - especially as Sale Water Park tram stop is nearby as this would make a good accessible walking route.
Would like the bridge to be suitable for horses to cross
I like the old bridge, it is a shame if it is removed. I agree that a more accessible crossing is needed.
Needs to be made horse friendly
PLEASE MAKE IT SUITABLE FOR HORSE RIDERS AS WELL AS BIKES
Please can horse riders be allowed to use the new bridge.
Although I'd prefer the original bridge remains in addition to the new bridge
I would like the old bridge to be left in addition to the new if that is possible, as it is an old historic bridge. Failing that, could the old coble ramps be kept on Jackson' boat side of the bridge?
The arguments put forward to support a replacement bridge are sound – the improved width provided by a new bridge should substantially avoid conflict between different user groups.

Table 5: Additional Comments given in relation to Question 6 of the online multiple-choice questionnaire

No strong preference
Whichever is cheaper to maintain long-term.
Both bridges are fine. Go for the cheapest and use the savings to improve access. Avoid sharp corners on ramps for turning.
Unable to comment - I don't know enough about it.
Pedestrians never stick to pedestrian paths or respect cycle lanes so you might as well make it all shared use...
Butterfly arches are much more aesthetically pleasing
Is it possible to copy the look of the old bridge as closely as possible in order to retain the heritage? In the images of the Southern entry of the bridge, the ramp looks a bit narrow, with a tight turning and with a muddy run up to it. Hopefully that will be addressed
Don't change the present one.
Neither
There really is no need to change it!
Only due to safer lower sides. Apart from that I have no preference.
Which is the most suitable for horses, pedestrians and cyclists.
Should be wide enough to have segregation of cycles and pedestrians
2 is incredibly ugly and not in keeping with the location
Unable to comment
As long as its horse suitable I don't care.
Only in addition to the nineteenth century pedestrian bridge.
Please ensure it is accessible and safe for horses
Is this one big enough for horses?
Any that can also accommodate horse riders
Bridge with a ramp and no steps.
Either as long as high suitable for horses
Neither I am as a resident in Chorlton totally incensed by this proposal. I use the old bridge daily and am aware it needs repairing but that is quite easily addressed .
As long as horses can cross safely
Must be safe and suitable for horse riders
Whichever is most suitable for horse riders.
Must be suitable for use by horses
Whichever is more Suitable for horses
Don't know the detail
Don't mind as long as it's horse friendly
The sides are high enough to accommodate horses and riders moving across the bridge
Either works for me. Please make it horse friendly
Don't know the difference and not clear in information provided.
See above. It's all biased towards too few user options.
I am not an engineer so I prefer not to comment
anything safe for horses
Curved parapets
no opinion
Functional and purposeful rather than artistic
Whichever is suitable for the most present and future users - see comment to Q5 above.
Higher sided

<p>Bridge Design</p> <p>There are 3 leaflets re bridge design on the attached link :- Bridges, Dimensions & Multi-User which may be useful and I'm sure that the Environment Agency will be aware of the dimensions needed to accommodate multiple users.</p> <p>http://www.bhs.org.uk/advice-and-information/riding-out/free-leaflets-and-advice</p> <p>The turning from the paths along the river meeting the bridge at Jacksons Boat is at right angles with a steep cobbled slope off the bridge directly into the pub car park. Moving the bridge 10m south brings it much closer to the pub walls and possibly less room. The material on the bridge and the sloping ramps would need to be horse friendly and wide enough to accommodate horses and turning on/off the bridge. One solution would be to dismount to cross the bridge.</p>
Safe for Horse riders to cross, not too low. A bridge that should a horse spook, the rider cannot go over the side.
Horse suitable
Butterfly Arch (but not at the expense of quality routes and surfaces elsewhere. The cheapest option should be chosen if any of the routes is at risk including Ashton on Mersey to Urmston Link an Amey Transport committed scheme see answers to Q1 and Q2)
As long as the surface is non slip and there is plenty of head room either bridge is fine.
Not having any expertise on bridge construction, I have no opinion to offer.
Which is more safe for cyclists?
I only saw a picture of the butterfly arch.
Unsure if these bridge options as unclear
No idea what these bridge designs are
Neither. No new bridge.
Warren Truss would be more in-keeping with surroundings.
none
Unsure of details. Unable to answer.
Both are distasteful and will ruin the aesthetics of the area. The existing steps and cobbles are very old and should be preserved.
alternatives for horse riders to be able to avoid the fastest bike routes would be good. If Hawthorn lane is to be a main bike route then it would be ideal if horses could go on the perimeter of Turn Moss ie at the other side of the trees and away from the fast traffic.
Don't need a new Bridge
Not sure what these are but horse friendly
No preference
Signage to instruct users to keep left and dogs on lead over bridge, to ensure less conflict with cyclists.
See comments above about the ramps and railings on either side. The existing cobbles on the pub side are also attractive and should be retained for aesthetic reasons if possible.
Butterfly arch over fussy /over engineered
Our preference is to replace with the bridge closest in design to the current bridge, out of the two options given we feel this would be the Warren Truss Bridge.
It really needs to include equestrians, I can't understand why it wouldn't
I have no preference but they must be accessible to horses
Don't mind as long as suitable for horses
My preferred design is something much closer to the current design.
Will feel less enclosed
I DON'T KNOW WHAT THEY ARE BUT PLEASE MAKE IT SUITABLE FOR HORSES
Looks more connected with heritage, rather than the butterfly arch that is quite jarring.
Don't really mind
Please could disabled ramps be more attractive and in keeping with the fact that this is a green space
The butterfly arch option may have some visual appeal but has a much bigger visual footprint. The traditional Warren Truss is a much neater and less obstructive solution.

Table 6: Additional Comments given in relation to Question 7 of the online multiple-choice questionnaire

The visual implies a zigzag slope for access. It should be a straight line slope from the road to the bridge, especially on the Sale Moor side.
Unable to comment - I don't know enough about it.
No need to change the present bridge.
Retain the old bridge and build the new one further away
Unable to see the plans clearly enough to tell. See above re siting away from the old bridge.
Generally as long as it is built prior to removal of old bridge OR could old bridge be kept and included .
Please please make this a multi user track, these routes are needed to get horse riders on safe routes like cyclists and give wheel chair and families another place to go
Involve horse riders
Please reconsider.
As long as it is Suitable for horses
Don't know the details
Please make it horse friendly
See above
Again no comment
why not have some small jumps?
No comment. What cost did you bid to do this?
The location should be chosen to provide the best options for access for all present and possible future - particularly on the south side, where there are the most constraints.
Could clearly defined cycling and walking lanes be included within the proposals?
See comments above
as above
Horse suitable
Not too close to the pub so that use of it impacts with the pub's clients.
See Q6.
Only if there is no other option
I could not really tell exactly where it was located. I hope that it is next to the existing bridge.
As above retain the old bridge build new one further toward the hawthorn lane end
Please make all routes accessible to all - no barriers or gates.
Unable to say without links to full details
Please give special consideration to the link route through Turn Moss as outlined above. This is the perfect complement to the existing route and will really encourage the use of walking and cycling on longer routes from North Trafford and Chorlton towards the Transpennine Trail. I assume this would have been included in the original plans, were it not for the projected development of Turn Moss, which has now, of course, been abandoned.

Table 7: Overall additional Comments given in relation to the online multiple-choice questionnaire

<p>To encourage new cyclist to access the routes Dog walkers to be aware that social cycling is also a form of exercise and for mental health and keep dogs under control. Speed cyclists need to keep to a safe speed on cycle paths !</p>
<p>As a cyclist the key improvement to my mind is the access ramps, the current bridge has quite poor on/off ramps, particularly at the Jackson Boat side where it is very steep.</p>
<p>The Jacksons Boat bridge in particular should improve my access to Sale Moor to access shops and other services.</p>
<p>I hope this is the start of much larger programmed scheme of cycle infrastructure work across Trafford to promote more cycling</p>
<p>Great to hear about this investment in walking and cycling, please keep it coming. If the facilities are there people will use them. Thank you</p>
<p>Cyclists get all the support these days when it comes to improvements and all to help them go even faster than they already do Many cause a hazard from riding too fast up behind horse riders or speeding towards us Some are polite and some are rude and tell us we shouldn't be there! It would be good if signs could be erected to show that it's not just a cycle route!! Please take horse riders into account in all your improvement plans Thankyou</p>
<p>Please consider spending the money on improving the biodiversity of the Mersey Valley - which continues to decline year after year.</p>
<p>My conclusion is some limited upgrades necessary, particularly in the direction of Urmston from sale water park. In general existing infrastructure and paths are generally adequate in both wet and dry weather for both cyclists and pedestrians. If you are doing the Urmston extension then you really should plan a route that actually gets you near Urmston rather than throw you out on a busy road with still some distance to go.</p>
<p>A priority has to be that this is a safe and accessible route.</p>
<p>Whilst I understand and appreciate the desire to motivate folks to be more active, I am disappointed that cyclists are taking priority. Apart from health and safety issues , horse owners contribute at least £2000 per horse per annum to the local communities which is far more than cyclists</p>
<p>Please not that the type of surface is very important not only to cyclist but also to horse riders and dog walkers.</p>
<p>Equestrian users should also be considered. This is a popular healthy sport. Cyclists and walkers can also use the route. Why not let horses use it???</p>
<p>open more bridleways most are still there, to many horses and riders getting killed by impatient drivers</p>
<p>We box up from where we live to ride this area (park at a friends yard) so how about some parking</p>
<p>I don't use the area regularly but have ridden parts of the route - especially the trans pennine way & appreciate the importance of routes which are accessible to all</p>
<p>You incorporate 2 bridleways in this plan yet do not mention anything about including horses in this proposal. This is not acceptable. You can't take a bridleway from us.</p>
<p>Greater Manchester is losing too many of its historic structures, and becoming bland and soulless. The old bridge at Jackson's Boat dates from the 1880s, is picturesque and fits in with the nearby public house. It should be saved for pedestrian use. I am generally in favour of encouraging cycling but am concerned about fast cyclists using such well-loved walking routes - measures to slow them down/segregate them from pedestrians would be appreciated. I am also concerned that trail bikes will take advantage of an improved route. They already create a nuisance in the Mersey Valley from time to time.</p>
<p>Thank you ,</p>
<p>ASAP please</p>
<p>Please consider all used, use the money to help as many users as possible be able to access these routes and enjoy the area!</p>
<p>I am a horse rider and would like you to cater for us too please!</p>
<p>If it is to be multi user friendly than you must consider horse riders</p>

Please ensure that access and surfaces can accommodate horse riders. Roads are increasingly dangerous to ride on. These green spaces and access are vital
Please ensure that the improvements made are appropriate for horses and riders to enjoy safely. Riders are increasingly forced to ride on roads which can present unpredictable dangers to both. Horses enjoy their rides out too. Some horses can not be ridden in a ménage for their health and need to exercise on a ride out. Please consider the animal welfare aspect.
All proposals should also accommodate horse riders too
Although I don't use these routes on a regular basis I think it is important to consider horses and riders safety - with the loss of many bridleways horses and riders are either having to take their lives in their hands or not go out at all. Please consider horses and riders in your plans.
I am not local but may at some time in the future take part in an organised ride. I'm filling in this survey to support fellow riders in your area as we are nearly always missed out of plans for off road route. Our ever decreasing bridleways are often hijacked by other users and adapted for them without any increased provision for horse riders.
Horse riders appear to have been forgotten when many of these routes are used by horse riders regularly and two are existing bridleways. Horse riders must be included!
I regularly ride my horse along this route. There is no provision mentioned for horse riders? I assume this route will still be accessible? We need safe places to ride off road away from traffic.
I would say most people I.e dog walkers, truckers and cyclists whom I have talked to were not aware that Trafford A. Seem to have say in the destruction of this iconic bridge and B., think the councils have dealt with this in a very underhanded way. I seriously doubt that the council that takes this on may not get reappointed if it does not listen to the Chorlton residents on this matter. I am not alone in this view.
This is a lovely area where many people enjoy riding their horses, and all provisions should be suitable for horses. The percentage of off-road routes available to horse riders is tiny compared to those for cyclists and walkers.
Like I said before, I have no problem with development of cycle routes as long it takes into account the others users ie horseriders on existing routes which this route would run into or take over. Make room for both and clear to all users who is allowed there and it wouldn't be a problem.
Please do not leave horse access out of the plans. We are already limited to where we can ride and the roads are not safe
As a horse rider I believe any new routes that are being planned for cyclists & walkers should also be open to horse riders. Without consideration of horse riders it is surely not a multi user route & would be effectively excluding this user group. Since this is a user group predominantly enjoyed by women & children, I feel it is discriminatory not to consider this group and while there is much consideration for male orientated sports such as football, rugby etc. there is much less funding & consideration for pursuits enjoyed by women. As a Council, I think you should be inclusive & consider all the population in their pursuits which improve their health & well-being such as helping to provide & improve safe off-road routes for horse riders.
There are many horse riders in this area using the existing tracks on which this route will be based and some of these are tracks are bridleways. Therefore, the route should also include horse riders. In addition, the route will run through a predominantly urban area and safe off-road routes for horse riding are essential.
Lots of horse riders in this area and they need to be taken into consideration not just cyclists
Please consider equestrians in this! I myself am I rider and know the limited access across the north west of paths we have so result to extremely dangerous roads which puts us under risk and has caused more rider deaths before. Keeping these as equestrian paths reduces that risks and saves lives for both horse and rider. These paths are frequently used by riders anyhow. Please don't force them onto the roads and limit there access we already have limited pathways in the north west already, thank you
Please give horse riders due consideration in these developments as we need more off road riding not less thanks

Please can you take into consideration access for horse riders. Aswell as pleasure riders there are 4 riding school businesses using these routes.
Many horse riders use the current routes alongside walkers and cyclists, all plans name walkers and cyclists as users but not horse riders. Many local businesses(equestrian centres) and private horse owners in the local area would be forced on to roads and unable to access the water park if the plans meant that horses were not allowed on the new paths.
Horse riding should not be forgotten in the group of proposed users. They have long used the tracks, paths and bridleways.
Please do not exclude horse riders from route improvement plans. Horses are being forced onto the roads at an ever increasing rate due to declining off road riding routes. This is a safety concern as riding on roads is not pleasurable and, more importantly, not safe.
Routes should continue to be all user access, not skewed towards one user group at the expense of another.
Although I am not particularly local I have enjoyed a couple of British Horse Society - organised rides around the Water parks and would definitely do so again. Please include horse riders in the named user groups! Kind regards
Please consult wider, equestrians and equestrian establishments are users of this network. Do not focus solely on the needs of cyclists
As stated, it's lovely that money is being spent for cyclists but not spent for route for horse riders, which provide work for many local businesses, farrier every 6 weeks £75 , stable fees £25 Pw: hay straw feed from suppliers £70 per month vet fees, insurance, etc how much does a cyclists give back to its community. I am a member of the BHS whom actively supports and encourages education on horse welfare and road sense. I'm not local to this area but I've seen the same done to my local area old rail way lines converted to tarmac for cyclists, fields tarmac for cyclists, parks tarmac for cyclists making more horse riders go out on busy roads to find alternative routes
The new route must be suitable and safe for walkers, riders and cyclists. To use footpaths and bridleways in the creation of this cycle route and not provide access to horse riders is not acceptable. As a walker, cyclist and horse rider I think this route should be enjoyed by all as a public bridleway linked to other sections of the TPT.
no preference
The equestrian industry is an important source of income in the rural economy – diversification for farmers by providing livery facilities, farriers, instructors, feed merchants, riding schools, vets etc. This industry needs safe paths and tracks for riders to use. Failure to provide these facilities will impact on the rural economy opportunities in the area. Therefore the proposal to exclude horses fails NPPF 2018 Supporting a prosperous rural economy - b) the development and diversification of agricultural and other land-based rural businesses; 2.4 In respect of Transport, Section 9 of the NPPF relates to 'Promoting sustainable transport' HE's own Accessibility Policy includes: Highways England operates, maintains, and modernises the strategic road network. As part of this challenge, we're delivering an £11bn investment programme to support the government's Road Investment Strategy. An accessible, inclusive and integrated network is a key part of delivering an effective transport system. Our ambition is to enhance accessibility to and across the network for users and communities, as set out in the Highways England Delivery Plan. Our vision for accessibility Our vision focuses on supporting our road users' journeys, pedestrians, cyclists, equestrians, those with disabilities (such as users with mobility or sensory impairments) and other vulnerable users – while delivering longer-term benefits for communities and users alike. The proposal to create this route which uses bridleways but excludes equestrians from large parts of it fails to meet Highways England's own policies and therefore should be reassessed to include equestrians.

Highways England / Trafford Council should immediately consult with the British Horse Society who represents the interests of horse riders.

I'm in favour of improving access but don't want to lose bridle ways.

Please consult as a priority with the local branch of the British Horse Society, don't just focus on bikes, horses need access to .

I would like to re-iterate my concerns over the safety aspects associated with these proposals. in particular both speeding and , inappropriate surfaces can cause problems for riders and for others in their vicinity.

I would not be happy if an upgraded route resulted in a deterioration of facilities for other people who walk on a regular or irregular basis. Trafford has active groups promoting Walking for Health where people who want take regular exercise meet as a group and walk along paths in all areas of the Borough. On the Bridgewater Way some of these groups are intimidated by speeding cyclists. It would be unfortunate indeed if the proposed routes are upgraded and then become a 'no-go' area for walkers.

Removal of horse stiles whilst facilitating access would (in my view) encourage access by off road bikes. I'm more than happy to get involved/advise from a horse riders perspective. There is huge potential here for the Council to generate income from visiting & local horse riders.

Make everything suitable for me and my horse to use. Thanks

Some sections of the routes are dark and need either lighting , stricter seasonal cutting regimes or both.

Section 59 legal warning notices need to be posted liberally so that illegal motorcycling use can be dealt with swiftly and effectively)

We were promised a round cornered guard rail on both sides of the recently improved route were it crosses the Ousel and Kickety Brooks 53.4378 , -2.3239 . No guard rails have been installed here ! The proposed works on this section of the TPT should include this.

Any bollards should be positioned so as to leave a 1.5 metre gap sufficient to allow larger cycles with trailers and also to accommodate Trishaws (Cycling Without Age) .

The South East Access route starts from the Visitor Centre which is not an easy place to get to by foot or cycle . Improved direct access' are required to the Visitor Centre from both the tram stop and the shared footpath under the Mway at J6.

All routes should have seasonal cut backs regimes that clear the path by 2 metres beyond the path width on both sides where possible. The section of recently improved path on the East side of the A56 has shrunk from over 3 metres to less than 1 metre in less than 12 months !!!

Mersey Sale Water Park South Proposed Works Sheet 4 of 4 appears to show a walkway only 1.2 wide . This needs to be 1.5 m wide .

The existing footbridge parallel with Carrington Spur. Can this have a name plaque and a name that

describes its location or history rather than the unhelpful and informal Millenium Bridge ?
Mersey Bridgewater Canal Proposed Works Sheet 1 of 1 shows a Chester style polyurethane bollard with sign face in the key . Where is this/these to be located ? What will be the text on here ?
Please consider horse riders when planning and executing these proposals. I regularly ride on the River Mersey.
Thanks for allowing the opportunity to comment on the proposal. The hardcopy paperwork including colour photos that was provided at the visitor centre was excellent and descriptive. I did get the impression that many visitors particularly regular users may have missed the scheme exhibition in the visitor centre as it appeared to be tucked away. It was the sensible location to have it, however most regular users will just make a beeline in the direction where they want to walk etc. I can't remember but there may have been signage inviting people into visit the exhibition and I just missed it. Best wishes with the scheme. Lets have more! ??
Please make these paths multi user. Horses use them now and don't want anything special apart from a safe non slip surface.
These paths should remain a resource for everyone alike and cyclists need to respect that they are sharing paths with walkers and horses. The Bridgewater towpath is already too dangerous for pedestrians, especially families - we don't need to see this spread to the rest of the area. If you really want to improve these paths, put in more barriers.
Whilst I don't live that close I would travel to ride. and I am concerned that the needs of horse riders are not given the same recognition as bike riders
I think the most important thing is signage and surface, I would like a cycle route to Partington from Broadheath as Sinderland Lane is incredibly dangerous, Turn Moss needs a new path from Edge Lane again with signage, as a cyclist I am in favour of any cycling infrastructure we get and hope the Sale to Urmston route opens up very soon for all bikes not just mountain bikes!
I think we need to see investment into a number of "shared" pathways,between walkers and cyclists. New walking routes and cycle routes are much welcomed.
Would appreciate that footpaths have a designated cycle lane separated on them as a regular Walker who is partially deaf I find cyclists a bit of a nightmare as they automatically presume you know they are there and I have had a few near misses and a collision as I didn't hear them coming. There are more.people than is perhaps realised that this problem applies to
What budget provision is being made to ensure this new highway is kept in useable condition in the years ahead, by when it may have become part of an essential travel neteork
Please look at wider links to other area of use by walkers and cyclists like Turn Moss Longford Park etc. This is focused too much on the east west nature of the route without a north south view to access it.
I am not local, but I have previously used these routes to get from my home in SK8 to Urmston Grammar. This remains a frustrating & muddy experience. Please ensure this scheme allows cyclists to traverse the entire route without being forced to dismount! Please also make the surface weather proof. This is Manchester! Gravel (loose aggregate) is not good enough.
Please remove all gates and barriers. These routes need to be accessible to all - wheel chairs, adapted bikes, trailers, tandems etc.
I always thought it was a missed opportunity not to have added a seperate cycle path alongside the newer parts of the tram network that link Wythenshawe, Sale & Chorlton. There need to be improved, surfaced & lit pathways that feel safe enough for people to use all year round. I cycle between Sale & Wythenshawe & Cornbrook most days, until the winter when cycling along dark & icy canal paths (Sale to Cornbrook) just does not feel safe. I'm all for improved cycleways & would love to see Manchester develop along the lines of Copenhagen's cycleways, where the cycle paths are gritted in the winter, and with safe infrastructure & bridges like The Bicycle Snake (basically the Mancunian Way for bikes).
Use the opportunity to link the Mersey pathway with Turn Moss and leave the historic bridge at Jackson' Boat alone
Anything that impryand encourages more cycling is welcomed, especially if routes are fully linked and integrated into safe on road cycle paths and infrastructure allowing safe commutes for work, school college and leisure. Are these routes going to link to the BeeLines proposed routes?

<p>This area is one of the few unspoilt areas left in Chorlton/Sale. We already have to put up with the ugly team bridge... obviously no thought was put into that one. Enough is enough.</p>
<p>As an equestrian we use these routes often. It would be nice if we were considered in the proposal</p>
<p>Fantastic plans to improve our tracks but provision must be made for the large amount of horse riders within Trafford who use these tracks regularly.</p>
<p>Don't believe there's a need to spend all this money as people use the existing routes quite fine on their bikes at the moment and believe the monies would be better spent lowering the fares on the trams and charge for the parking and improve the rail network beyond the borders of greater Manchester so people don't have to drive here</p>
<p>I have listed the four routes I use most as priority 1, purely because I and most of the other horse riders are based in the Carrington, Flixton and Urmston areas and cannot use one section without including the others as part of a ride. Improvements along these routes would be really beneficial. I would like to suggest keeping the horse stiles at various points, which although may delay the cyclists slightly, will help to deter the motorbikes which are quite often a problem during the summer months. Providing horse riders access at Jacksons' Boat Bridge would be a fantastic opportunity to vary the existing routes available.</p>
<p>Main issue is that much of this route becomes boggy in the wet weather, anything to improve this is appreciated. Also I can see that it is proposed to cut back on vegetation, this too is much needed in the short term</p>
<p>This whole consultation has been badly publicised. Calling it Urmston to Sale Water Park cycle route improvements is giving the wrong impression. This is a series of improvements which cover the whole of the Mersey Valley from Carrington to Manchester (at Jacksons Boat). The title of the scheme should have reflecting the huge scope it covers. As it is, many people will not be aware of what this proposal is offering. A huge number of people use Jacksons Bridge in particular, than should perhaps have had its own consultation? Very little promotion of this consultation from Trafford Council. Not sure on engagement with specific groups of interest such as GM Cycle Campaign, Trafford Cycle Forum, local bike shops like Ken Fosters and the cycle hire facility at Jacksons Boat.</p>
<p>The British Horse Society is happy to comment further but we have only just been made aware of the closing date, so this response merely sets out the principle that the British Horse Society supports the proposal on the proviso that the scheme is designed so that it accommodates horse riders as well as walkers and cyclists. At present the scheme only refers to walkers and cyclists which does not accord with highways England's Accessibility strategy. Please let us know if you would like us to submit further details.</p>
<p>This is a joint response on behalf of the three councillors in Chorlton, Manchester and is informed by the views we've heard of our local residents.</p>
<p>Please include equestrians on ALL of these routes, the roads are a really dangerous place to ride and quite a lot of people are starting to come along to this lovely area just for a peaceful stress and traffic free ride including myself. We are hoping that the TPT will be able to extend the bridleway (and therefore include cyclists) to Stockport in the very near future.</p>
<p>Whilst not living locally, I have previously been on pleasure rides (horse rider) along parts of these routes.</p>
<p>Horse riders need safe off road routes. We will box our horses to routes within an hour or so from our homes because we want/need to be safe and unfortunately have to travel to have this right.</p>
<p>All new routes build must be multi user not just for cyclists this offers the best value for money</p>
<p>Our roads are getting busier and busier so it's important that horse riders have access to off road riding.</p>
<p>I feel all proposals to encourage vulnerable road users to use off road routes is a fantastic idea but horse riders must be included in this plan. It would be very short sighted to discriminate against horse riders when there is an ever increasing need to keep horses off the roads.</p>
<p>The area is lovely for horse riding - would be great if equestrian users could be considered in the plans, please</p>
<p>Please Consider Wheelchair. Proposed plans look good to support this.</p>

<p>I am a British Cycling ride leader. We appreciate safe off-road routes for leading groups of cyclists on recreational bike rides. They are particularly Valuable for encouraging new cyclists and children to ride bicycles. I have used all of the routes under discussion.</p>
<p>It is very important that horses and their riders are able to use these areas.</p>
<p>I use the Bridgewater way at least 3 times a week and am very disappointed by the thoughtless actions of a small minority of users. Cyclists pedalling flat out past pedestrians. Dog walkers with extendable leads allowing their pets to roam. Walkers two or three at rest on the towpath blocking other users. There should be a code of conduct for users of the path clearly displayed on large notices. If there is sufficient room, the path should be divided between cyclists and pedestrians.</p>
<p>Reference has been made to the mixed indication of 'footpaths' and 'shared use paths'. The scheme is unfortunately entitled '.....Cycle Route Improvement Scheme' but must be equally attractive for use by pedestrians as well as cyclists – as stated, both are excellent means of getting from one place to another and the promotion of exercise is paramount and should be the main objective. Throughout the scheme's introduction emphasis has been made that the facilities should be for both cycling and walking.</p> <p>The path ways must be marked in such a way as to indicate the line to be taken by pedestrians and that to be taken by cyclists. To not have the surfaces marked in such a way is almost inevitably exposing the CRIS to the probability of conflict as has been found on the Bridgewater Way. One of the best examples of a well-designed and well executed shared use walk/cycle way is that constructed by LCC running from the centre of Lancaster to the seafront at Morecombe. The following is an extract from the Lancaster Guardian last year:</p> <p>The discipline of the white line on the Morecambe greenway is observed by most cyclists and pedestrians, and is helpful in this regard, so is probably worth instating in other areas. Mr Stubbins said the path is one of the major successes of the improvements at the end of the last millennium. He said that people should avoid adopting the thinking of 'cycling versus pedestrians' and should work together to improve the provision of off road routes. He added that consideration should be given to widening the paths in some areas, and that this should be something to bear in mind if and when plans to extend shared use paths come to fruition. Even here there have been isolated reports of conflict.</p> <p>It is noted that the CRIS pathways are all to be 3 metres in width with the one exception of section 07 which is to be 4 metres. The current pathways along this stretch is between 0.5m and 1.5m wide and I find it difficult to see the justification for this one length to be as wide a 4 metres – that is as wide as many a smaller residential road.</p> <p>Accepting that individual replies are not possible, always willing to partake in further consultation as there are more points that could be raised.</p>
<p>All Paths/routes should be sufficiently wide for pedestrians and cyclists to pass one another comfortably and safely</p>
<p>Please see previous comments about connecting up Longford Park / Edge Lane & Hawthorn Lane at Turn Moss.</p>

Appendix C: Comments from Public Consultation

(NOTE: All comments reported with the grammar, spelling and format in which they were received)

In addition to the formal consultation, a number of submissions were made via email, a summary of which is outlined in Table 8. It should be noted that all information documented in Appendix C were received prior to 25th January 2019 when the existing bridge located at Jackson's Boat was added to the List of Buildings of Special Architectural or Historic Interest by the Secretary of State for Digital, Culture, Media and Sport. The bridge is listed at Grade II.

Table 8: Content summary of submissions sent to 'One Trafford' during consultation period

Means	Date	Comments
Email	28 th October 2018	<ul style="list-style-type: none"> • Objection to shared cycle paths due to high speed of cyclists and the risk they present to pedestrian safety
Email	29 th October 2018	<ul style="list-style-type: none"> • Objection to shared cycle paths due to high speed of cyclists and the risk they present to pedestrian safety • Concern of small motorcyclists and 'muggers' on improved routes suggests the need for barriers
Email	31 st October 2018	<ul style="list-style-type: none"> • Objection to bound asphalt surfacing on cycle paths due to its encouragement of high speed cycling and the risk presented to pedestrian safety • Barriers needed to be narrowed to reduce high cyclist speed
Email	31 st October 2018	<ul style="list-style-type: none"> • Objection to bound asphalt surfacing on cycle paths due to its encouragement of high speed cycling and the risk presented to pedestrian safety • Unbound surfacing is better for pedestrians
Email	31 st October 2018	<ul style="list-style-type: none"> • Member of Sale Walking Group with strong objections to restrictions being lifted on footpaths due to high speed of cyclists and the risk they present to pedestrian safety
Email	31 st October 2018	<ul style="list-style-type: none"> • Member of North Wales Bridleways Association and BHS with strong objection to the exclusion of equestrians in the proposals • Request for safe areas for equestrians to ride off road
Email	31 st October 2018	<ul style="list-style-type: none"> • Objection to exclusion of equestrian access to proposed bridge design at Jackson's Boat
Email	1 st November 2018	<ul style="list-style-type: none"> • Objection to bound asphalt surfacing on cycle paths due to its encouragement of high speed cycling and the risk presented to pedestrian safety
Email	7 th November 2018	<ul style="list-style-type: none"> • Committee member of Greater Manchester and High Peak Ramblers with objection to bound asphalt surfacing on cycle paths due to its encouragement of high speed cycling and the risk presented to pedestrian safety • Request for barriers to be modified to restrict '4 wheeler mini motor bikes' who are very intrusive and dangerous
Email	7 th November 2018	<ul style="list-style-type: none"> • Suggestion of speed restriction barriers due to cyclists' excessive speed putting pedestrians' safety at risk
Email	7 th November 2018	<ul style="list-style-type: none"> • Suggestion of speed restriction barriers due to cyclists' excessive speed putting pedestrians' safety at risk
Email	7 th November 2018	<ul style="list-style-type: none"> • Strong objection to shared paths because of proven risk of collision and possible injury
Email	7 th November 2018	<ul style="list-style-type: none"> • Concern over equestrian exclusion

		<ul style="list-style-type: none"> Request for proposed surfacing to be like that of the Trans Pennine Trail
Email	7 th November 2018	<ul style="list-style-type: none"> Member of Trafford Ramblers club whom objects to the removal of barriers and restrictions to reduce excessive speed of cyclists
Email	7 th November 2018	<ul style="list-style-type: none"> Concern of use of bound asphalt surfacing on cycle paths due to its encouragement of high speed cycling and the risk presented to pedestrian safety, particularly elderly pedestrians and young children
Email	7 th November 2018	<ul style="list-style-type: none"> Pedestrian priority signage is an insufficient method for reducing cyclist speed. Cyclists' excessive speed puts pedestrians' safety at risk
Email	7 th November 2018	<ul style="list-style-type: none"> Member of 'Friends of Turn Moss' requesting the addition of a 150m asphalt path connecting between Hawthorne Lane and Turn Moss Car Park in proposed design
Email	9 th November 2018	<ul style="list-style-type: none"> Concern over high speed cyclists putting elderly pedestrians' safety at risk
Email	9 th November 2018	<ul style="list-style-type: none"> Concern over local horse riders' needs being ignored by local government and Highways England

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